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AUSTRALIAN AMATEUR
BOATBUILDER/ KITBOATS
CELEBRATES
100 ISSUES!

DIDI 40CR

DUDLEY DIX DESIGN

PASSION X

ISSN 1039-1657



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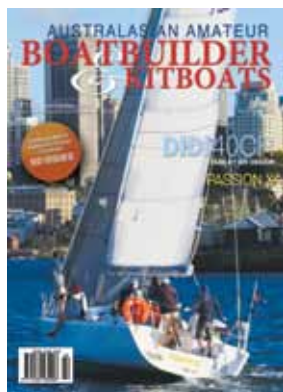
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FRONT COVER:

Passion X
Didi 40cr – Dudley Dix Design

Image Peter Miller



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Beautiful interior woodwork of Bill Connor's Didi
40cr Alizée. Top quality amateur woodwork.

DIDI 38 & DIDI 40CR

OWNER-BUILT BOATS WITH
GREAT PERFORMANCE

by **DUDLEY DIX**

Radius chine plywood, as a boatbuilding method, started as an experiment and developed over the next decade to where it became a major part of my design business. About half of my new commissions in the past 20 years have been for this method in one form or another, from small day-sailers through to large cruising catamarans.



Didi 38 *Bekhoir*, built by Stas Rechenkin, on Lake Baikal in Siberia, Russia.

I was the guinea pig in this experiment and I tested the construction concept and the boat thoroughly before we started to sell plans to others. I had sailed the Cape to Rio Race across the South Atlantic Ocean in 1993 aboard a GRP classic cruiser of my design, the Shearwater 39 *Ukelele Lady*, sailing as navigator and sailing master. At the time my own boat was *Concept Won*, a 34ft multi-chine plywood design that I drew in 1979 and built a few years later. During that Cape to Rio voyage I concluded that I needed (wanted) a new high-performance boat for the next race, to sail as skipper.

Intending to build it myself during the intervening three years, I started drawing the design for strip cedar construction. But the funds to finance the boat eluded me until commissions came in for two big designs that freed enough money for my build to

start. By then nearly a year had passed and I doubted that I could complete a strip boat in the remaining two years, working mostly solo as an amateur in my garden.

Prior to this, I had drawn a series of radius chine steel cruisers for local boatbuilder clients and decided to try something similar with plywood, to speed up the build. A big difference was that for an equivalent length of plywood racer/cruiser, the displacement is less than 50% of the steel cruiser. That meant a much shallower hull that needed a different approach to hull form and the radius. The change of material also required development of new details to ensure strength sufficient to take on trans-ocean voyages in a very light boat, with everything that nature can throw at a small vessel on big waters.

Contrary to forecasts by professionals in the South African boating industry, I completed the boat in time to be on the start line in January 1996. It was tight but we made it, thanks to labour input from crew. Total build time was approximately 3000 hours.

Testing the structural details and the overall toughness of the boat is what I did by racing her with a full crew the 3500 miles from Cape Town, South Africa, to Rio de Janeiro, Brazil. This race is typical

Our return voyage was a lot further south, still counter-clockwise around the South Atlantic High, skirting the edge of the Southern Ocean. I did this with one crew and a tiller-pilot to back us up at the helm. Unfortunately, the tiller-pilot died a few days out of Rio, leaving the two of us to hand-steer more than 3000 miles to get home. This voyage takes boats into the clutches of the endless procession of cold fronts that go west-east around the bottom part of the globe,

so we found ourselves in numerous filthy storms en route. They helped us to hone our heavy-weather skills, from sailing around big holes in the surface of the ocean, to heaving to when I felt that to be the best tactic. They taught me how a light boat needs to be sailed in tough weather and big seas and also just how well a modern boat can look after itself and its crew in storm conditions.

The boat that did all of this for me is the 38ft *Black Cat*. Named and liveried for South Africa's favourite peanut butter brand, it has proven to be a wonderful name for a yellow monohull. I skippered her across the South Atlantic three times. She is now 22 years old and has crossed the Atlantic six times in total, as well as thousands of miles in coastal racing and cruising around the Cape of Good Hope and three voyages between Cape Town and St Helena Island.

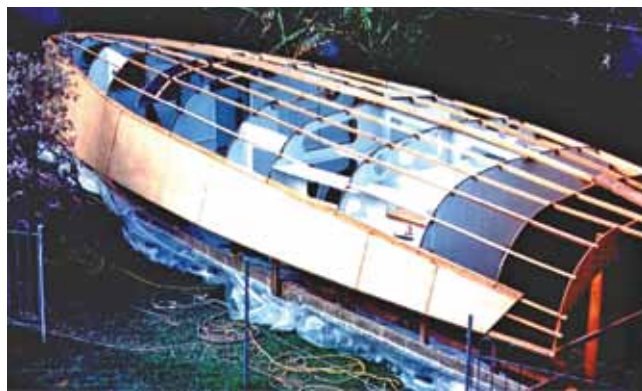


TOP: The launch of Didi 38 #1, *Black Cat*, after two years of building.

ABOVE: *Passion X* preparing for launch in the workshops of Woolwich Dock in Sydney. The owner and a professional boatbuilder friend installed the keel, with the yard doing the heavy-lifting. The yard sprayed the bottom paint.

downhill tradewind sailing, counter-clockwise around the South Atlantic High. The fleet generally gets caught by one big storm from a cold front within the first few days of the start, with occasional tropical thunderstorms further into the race. The sailing typically ranges from almost drifting through to 20+kt surfing, as well as being flattened sometimes when nature feels that we need to be taught a lesson in respect.

The hull form is a basic V-bottom hard chine, with the chine rounded off to a variable radius that is sized to suit what I need from that particular part of the hull. The resulting shape is not much different from a modern light-displacement composite hull. It has about $\frac{2}{3}$ of the hull area as flat sheets to side and bottom panels, which are skinned very quickly and are self-fairing due to the nature of the material. The remaining $\frac{1}{3}$ of the hull is skinned in two layers



of plywood, applied in strips 250-300mm wide. The junction between the flat and radiused panels is made over a stringer, with a plywood doubler to add bonding surface area.

The hull is built over permanent bulkheads, with the stringers slotted into the bulkheads and the skin bonded to the bulkheads with epoxy fillets both sides. The bulkheads are set up on the building stocks as permanent structure, with minimal wasted timber in temporary framing, keeping costs down. The backbone is a hardwood I-beam with transverse laminated floors to spread the ballast loads into the hull.

ABOVE LEFT: *Black Cat* hull skinned. The flat sheets to side and bottom are $\frac{2}{3}$ of the hull surface, linked by the radiused areas done in two layers.

ABOVE RIGHT: *Black Cat* hull with the sides skinned. Much of the interior was built before the skin was started.



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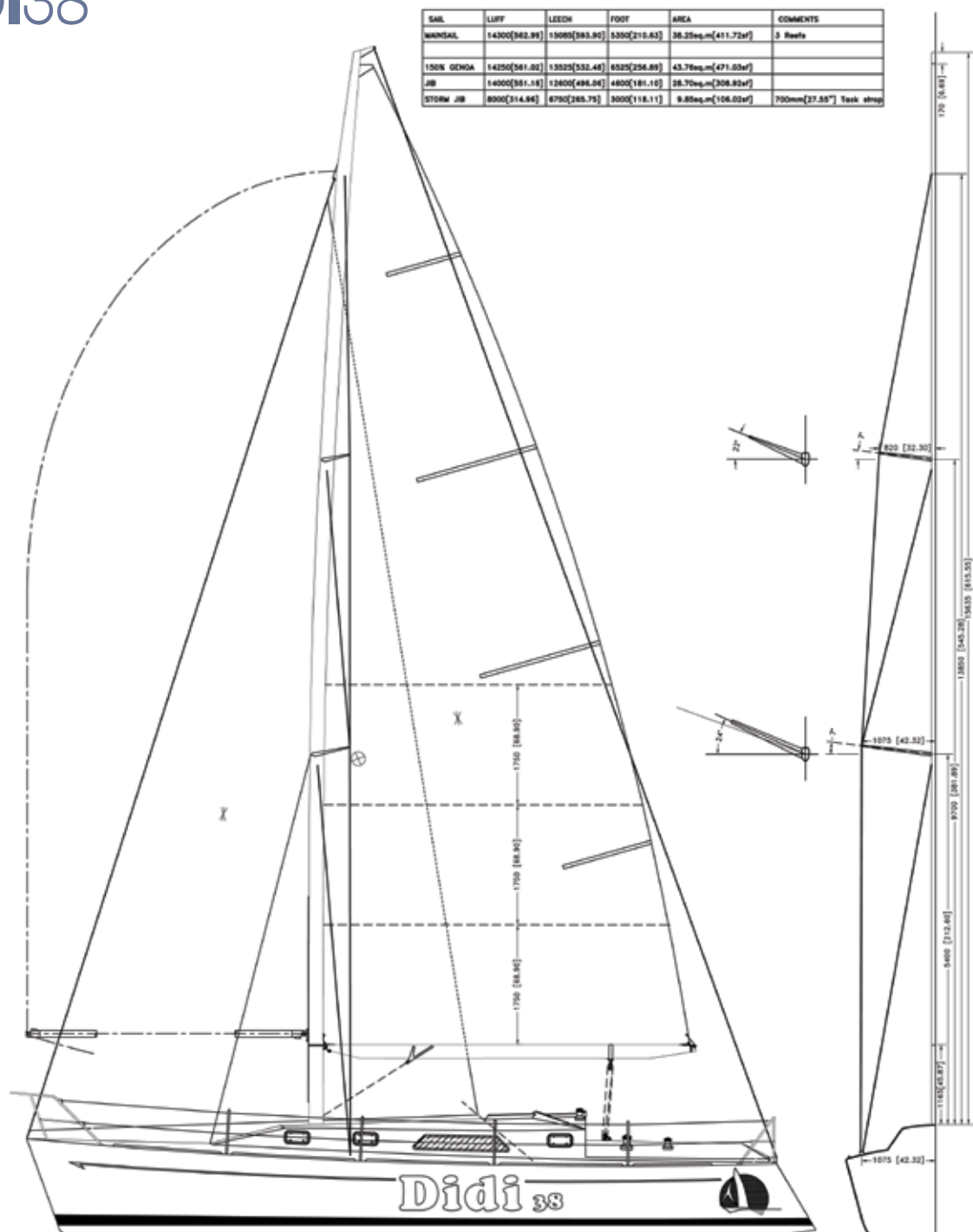
The deck and cockpit are all 12mm plywood sheet, also over longitudinal stringers that are slotted into the bulkheads. This is a much faster and easier way to build a deck than traditional transverse deck framing. The cabin roof is laminated from two layers of 6mm plywood, over transverse laminated beams.

The rig that I chose for my boat was deck-stepped fractional Marconi with two sets of swept spreaders. Underwater she has a fabricated steel keel with delta bulb, 2.25m draft, and poured-in lead ballast, with a

50% ballast ratio in lightship trim. For steering I went for my favourite arrangement, a balanced transom-hung rudder and tiller – simple, light, direct and easily serviced.

Black Cat didn't disappoint with her performance, surfing at 22kts on occasions and recording a 24-hour run of 250nm during her first Cape to Rio Race. In club racing she has been very quick on all points of sail in light wind flat water conditions and excels reaching and running in strong breezes.

DIDI38



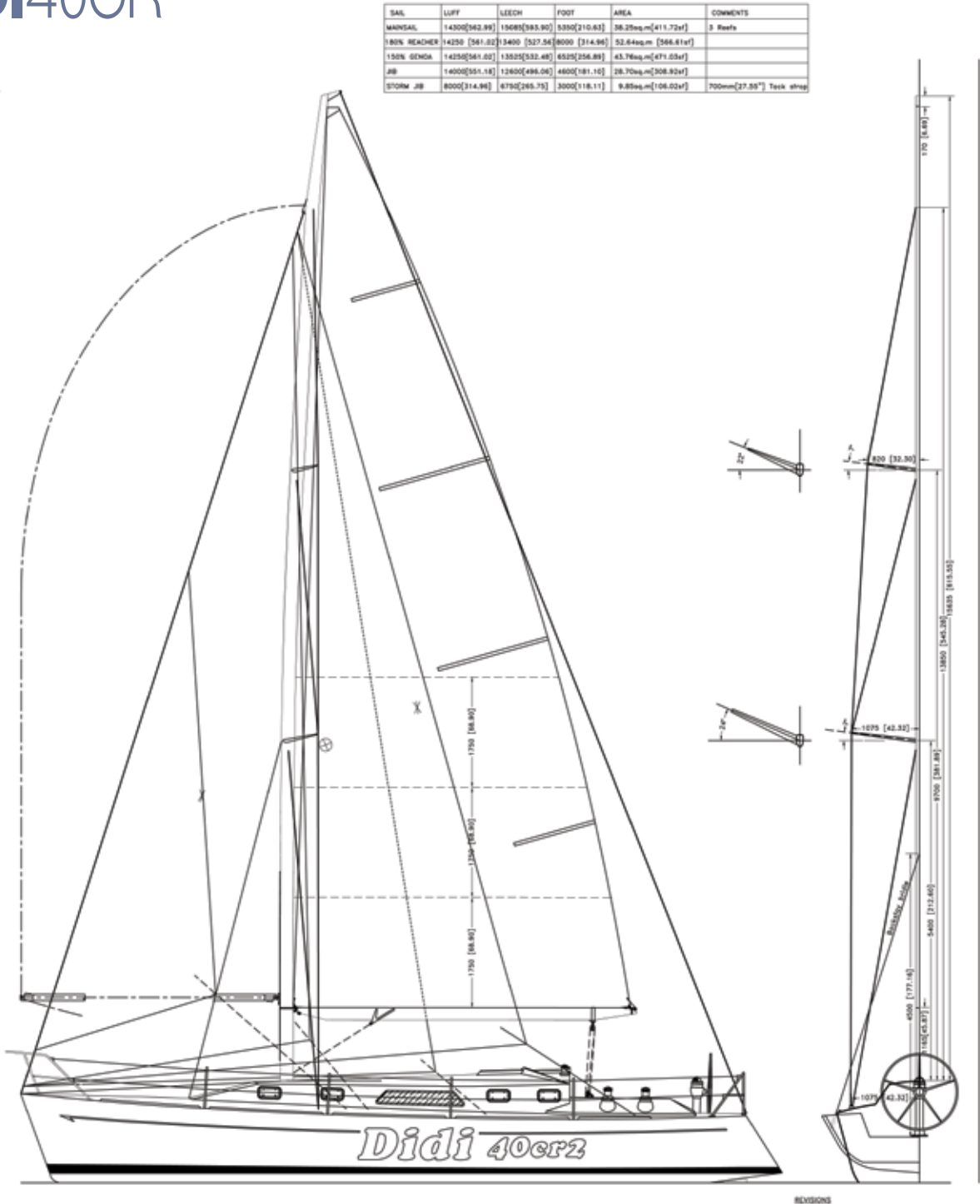
REVISIONS
 A) Chimeplate base changed Jun'97
 B) Deck dimensions added Sep'98
 C) Mast section info updated Jun'06
 D) Battery added Jun'08

I named the design the Didi 38. I drew it to the 11.5m length limit that was permitted on my marina at Royal Cape Yacht Club, with short overhangs and a relatively long waterline. That combination suited my needs but I knew that other builders may want a longer stern overhang, to better optimise their boats for rating under IMS, the rating system at the time. For those builders I included the option of an extended stern on the lines drawing, to take overall length up to 12.07m. The extra length also gave a

longer cockpit more suited to crewed racing. That version is the Didi 40, built from the same drawings as the Didi 38.

Along the way various builders wanted rig variations and shallower keels of varying depths, or a cast lead torpedo bulb instead of the fabricated delta bulb on *Black Cat*. Not everyone wanted to race across oceans, preferring to cruise islands with shallow waters. An inboard spade rudder with tiller was added to the options, then wheel steering.

DIDI40CR



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150N GENOA	14250[561.02]	3500[136.82]	6500[255.90]	43.78sq.m[471.03sf]	
JIB	14000[551.18]	12800[496.06]	4600[181.10]	28.70sq.m[308.92sf]	
STORM JIB	8000[314.96]	6750[265.75]	3000[118.11]	9.85sq.m[106.02sf]	700mm(27.56") Tack strap

REVISIONS



TOP: Bill Connor built his Didi 40cr *Alizée* in Boulder, Colorado. Winter played havoc with the building program.

ABOVE: *Passion X* relaxing at anchor in Lane Cove River, Greenwich.

The interior layout of the Didi 38/40 has three double berths, one in the forecabin and two in the quarters under the cockpit. These work well as singles at sea, each with a centreline lee-cloth separating a sleeping crew from tog bags and other gear on the other half of the berth. Settee berths in the saloon provide extra berths for racing with a full crew or short-term cruising with a full boat. Cave lockers in various places around the boat give plenty of storage spaces to keep things neat and tidy at sea.

It didn't take long for builders to start asking for a Didi 40 with the extra length going into the living accommodation instead of the cockpit, with a longer cabin and the shorter cockpit of the 38. They also wanted the heads to be moved aft for more comfortable use at sea. The resulting boat was the Didi 40cr, with the 'cr' denoting 'cruiser'. Not that the 40cr suffers from loss of performance, the cr just seemed a convenient way to tell the two apart.

One boat that included a few of these variations was built by Bill Connor

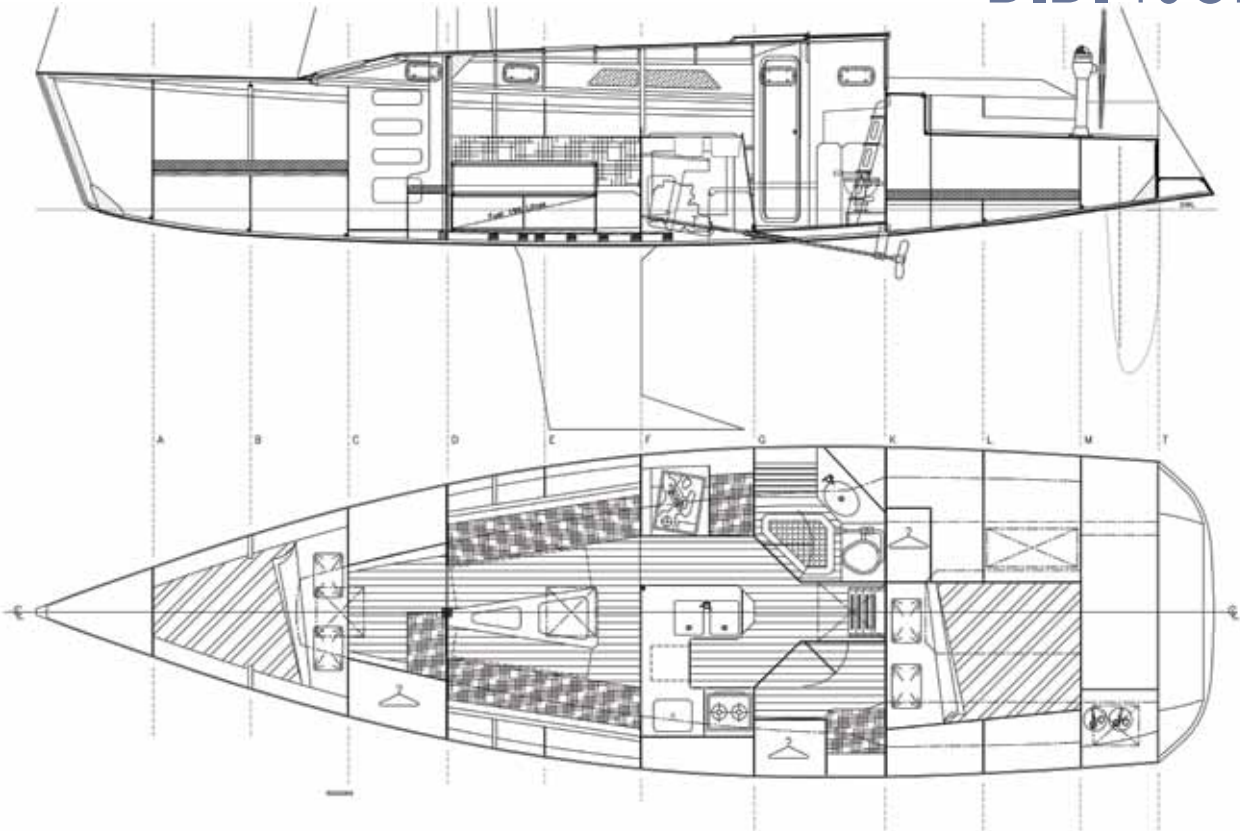
SETTEE BERTHS IN THE SALOON PROVIDE EXTRA BERTHS FOR RACING WITH A FULL CREW OR SHORT-TERM CRUISING WITH A FULL BOAT

and his family in the driveway of their home in Boulder, Colorado. That is in the foothills of the Rocky Mountains. When complete, they towed their boat, *Alizée*, more than half-way across the continent on a custom trailer, to launch in Connecticut. The purpose of this 2000-mile road trip? To exhibit their exquisitely-detailed boat on the Wooden Boat Show at Mystic Seaport.

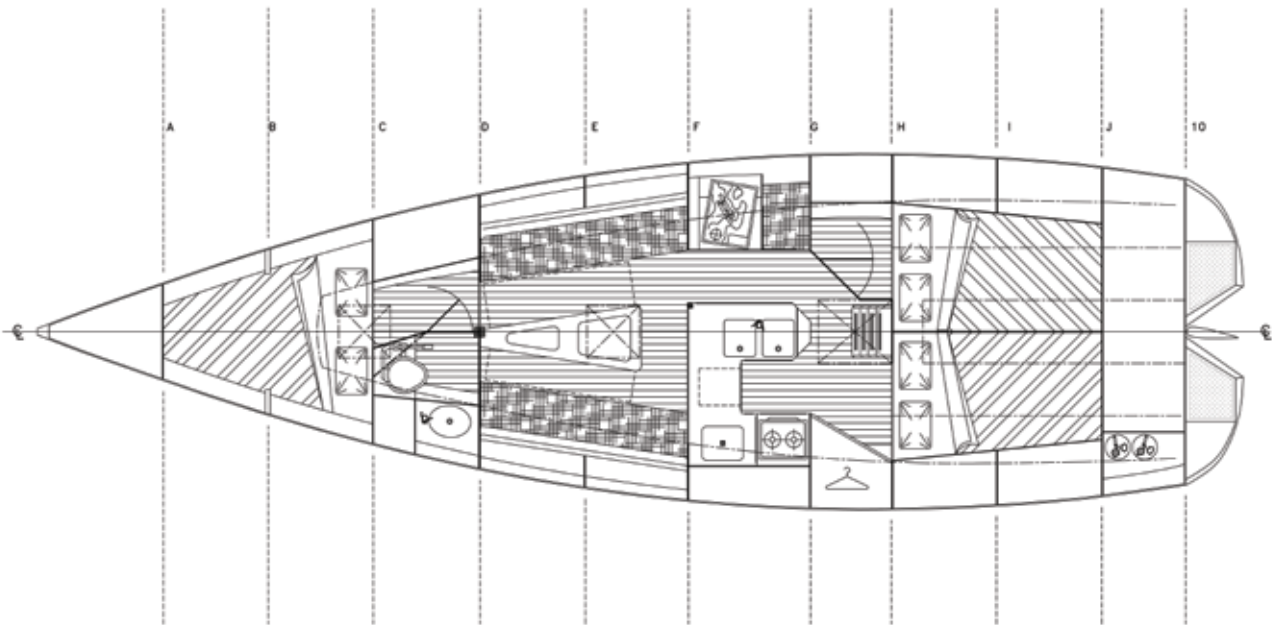
During building Bill and I had discussed the wheel/tiller steering options and I had said that *Black Cat* is such a delight to helm with a tiller that I would never consider a wheel for her. After his first sail on *Alizée* Bill sent the message "Why would anyone want a wheel on such a sweet ride?" His wife had been apprehensive about helming with a tiller

ACCOMMODATION

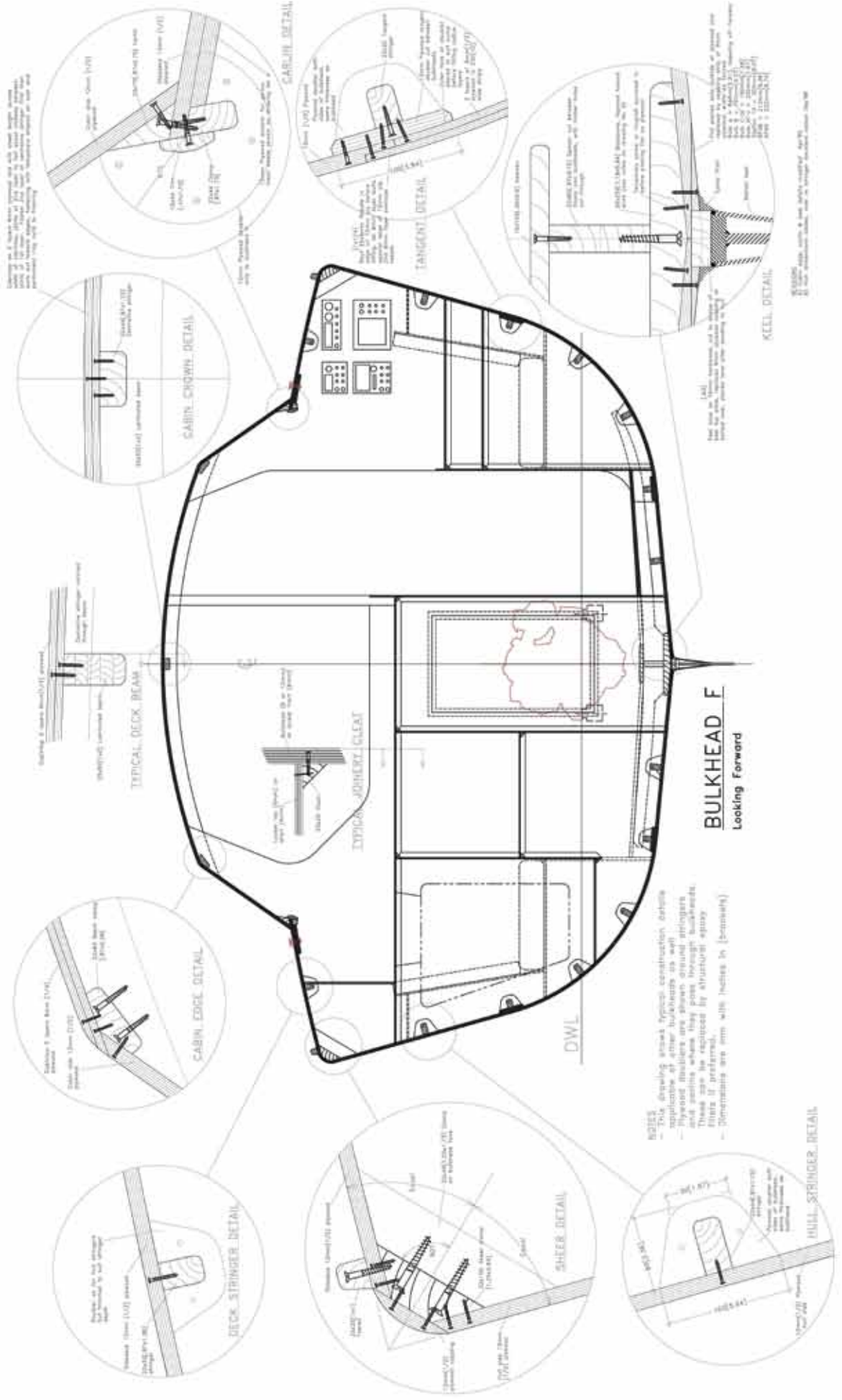
DIDI40CR



DIDI38



CONSTRUCTION DETAILS



but that first day of sailing he hadn't been able to get her to give him time at the tiller.

More recently I was contacted by David Edmiston of Sydney, who had seen the design series and wanted to build a Didi 40cr for himself, for club and ocean racing. But he wanted some modifications, to modernise the 20-year old design. He was the first builder to ask me to increase the keel depth, rather than reducing it. This was to go along with another first-time request, to increase the rig with masthead spinnakers and bigger mainsail. The deeper keel translates into larger loads, so the structure was beefed up. The hull and deck also went through changes, adding flare to the hull aft, increasing deck beam aft of Bmax through to the transom. This gave the space needed for a wider T-shape cockpit that can take a large wheel. David chose an inboard

rudder with tiller for his boat, named *Passion X*. The modified boat became the Didi 40cr2.

David built her in his garden, alongside his Sydney home, over three years. She was lifted over his house by a large crane, from her sheltered build location. From there she was transported by flat-bed trailer to a professional yard by the water for fitting keel and rudder, as well as bottom painting before launch.

The most recent variation that I have drawn is for a lifting keel. This option uses the same basic structure as the fixed keels, further reinforced by a laminated ring frame to support the front of the keel casing and

BELOW: *Black Cat* at the start of the 3500 mile 1996 Cape to Rio Race, sailing out of Table Bay at 10kts.





considerably more timber to the backbone structure to compensate for the big hole through the bottom of the boat. It uses a steel casing that transfers the keel loads into the timber structure. Lifting/lowering is by a Spectra rope that runs through sheaves bracketed off the top plate of the keel and the underside of the cabin roof. The line is led to an electric halliard winch on the roof, replacing the manual winch of the fixed keel boats.

Black Cat and her voyages attracted the attention of people wanting to build racers or fast cruisers for themselves. Plan sales to date are approaching 100 boats, most of them by amateur builders. A wide range of commissions came over the years, all inspired in one way or another by *Black Cat*. We now have 15 designs and variations that are built from plywood with radius chine methods, as well as one on the board right now and another two waiting in line. I have had requests this year for big sisters to *Black Cat* of 45,50,55 and 65ft but have had to turn them down until I can clear the backlog. There is so much interest in this construction method that I could draw nothing else and not run out of concepts.

TOP: Friends help with turning the hull of *Black Cat*. Much of the interior joinery was built before the hull was skinned, reducing fit-out time.

ABOVE: *Passion X* gets her first taste of salt water, in Woolwich Dock.

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PART 21

WANT A 50 GRAND SEA FISHING
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THIS IS HOW TO DO IT!



by **PETER CAPLEN**

Last issue we looked at finishing off the imitation teak rubbing band and the fancy toe rails with stainless steel cappings. This issue we are looking at the electrics and some cockpit detail.

Wiring a boat from scratch is not the epic task it may first appear as long as you don't try and do too much at once. When we wire a boat we do each circuit separately and label everything until it is all finally connected. This avoids confusing different circuits and ending up with bunches of wires and no idea what they are for!

The first step is to decide the position of the batteries. The engine start battery ideally wants to be as close to the engine as possible to avoid voltage drop when starting. The engine start battery for our project boat is mounted about half a metre away from the engine on the starboard side with the isolating switch mounted directly above it thus keeping cable runs to a minimum.

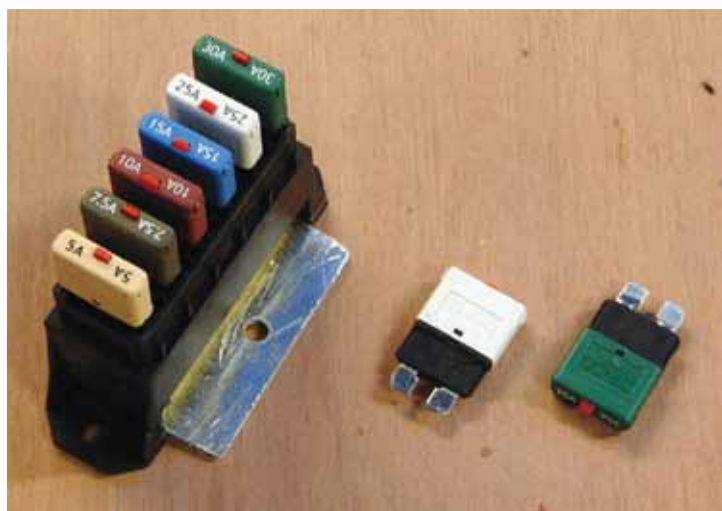
Unlike the engine battery the domestic batteries can more or less go anywhere that is convenient. This boat has three batteries; engine start, domestics and fridge. I know it is unusual to have a battery dedicated to the fridge but if it is used regularly it will quickly drain the domestic batteries and we have found from experience that having a separate fridge battery works well.



As part of the cockpit fit-out we built corner steps in each forward corner of the cockpit behind the wheelhouse. **(top)** The starboard one was an ideal size and position for mounting the domestic batteries. **(above)** The fridge battery is located beneath the domestic battery and is accessed by removing the shelf that the domestic battery is sitting on. (The port side step has been sealed and a drain added to use as the Propane gas bottle locker).

Once the batteries had been located the next item to be decided on was the fuse (or circuit breaker) locker and this is ideally placed somewhere near the helm and instrument panel. In this instance the ideal

location was in the large locker beneath the helm seat. To keep things simple and to lower the cost it was decided from the outset to use fuses rather than circuit breakers. Blade type vehicle fuses seem to be the most popular these days and as they can be bought for pennies were the ideal choice. However, we later discovered mini blade circuit-breakers that simply replace the blade fuses by plugging into the fuse holders so we later swapped all the fuses for these mini-breakers! (below)



Fuse holders are not expensive and come in various styles so we opted for six position holders with blade connectors that accept standard crimp terminal fittings. To ensure everything was fused properly we ended up with six holders giving us a total of 36 fuses. Even so we still had to double up some items onto single fuses. This is fine as long as the equipment to be doubled is of the same fuse rating and better still if it is items that are not normally used at the same time.

It is recommended to use tinned cable for marine use as this is more corrosion resistant than standard cable. However, I have always used standard car

cable and never had any problems with corrosion. Your nearest motor factors will be able to supply cable in large reels and the various cross sections you will need. The simplest rule of thumb when deciding the cable size for any particular use is "if in doubt increase the size". If the cable size is underspecified then voltage drop will always be a problem but if you oversize slightly then this problem will not arise. I recently bought several large rolls of cable from an eBay specialist making a big saving on the way. This particular lot was 'arctic' cable that is designed to remain flexible down to ridiculously cold levels and is more heavily insulated than standard two or three core cable. The only downside of this is that it takes up more room in the trunking when running through the boat.



With the batteries and electrical locker located running the wiring could begin. You may recall that early in this series during the initial construction phase we glassed-in several lengths of plastic water-pipe throughout the length of the boat to be used as trunking for wiring and pipe-work. The value of this unusual forethought could now be fully appreciated as wiring could be run to any part of the boat by cutting holes in the piping anywhere that wiring had to enter or leave and then using a simple wire pull-through to draw the cabling in and out of the piping. This was particularly handy when running cables from the helm position at the forward end of the wheelhouse to the engine compartment right aft.

Battery charging was dealt with as part of the wiring scheme and it was decided to use Voltage Sensitive Relays (VSR) for the split charging to ensure the batteries were separated when not being charged. This is an important factor in preventing the engine start battery inadvertently being discharged while using the domestic batteries. VSR's are probably the simplest and cheapest method of arranging a split charge system as they are connected between the engine start battery to which the alternator is first connected and each domestic battery. Unlike an ordinary relay that simply closes the circuit when power is applied to it the VSR will not close until the voltage in the primary battery (in this case the engine start battery) reaches a pre-set level of something 13.8 volts. This tells the relay that the battery is being charged and closes the circuit to allow current to flow to all the batteries in the system.

If the domestic batteries are very low after long use without charge then they may pull the engine start battery voltage down to below the cut-out level. In this case the VSR's will open and allow the engine start battery to build up its voltage level again before passing current to the domestic batteries. There is usually a delay built into the VSR to prevent it from rapidly cycling on and off in this situation.



Along with the VSR's each battery was equipped with an isolating switch to ensure that everything is totally disconnected when the boat is not being used. There are a couple of exceptions here as both the automatic

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bilge pump and the memory for the stereo radio need to be connected constantly when unattended. This means arranging a separate fuse board with a constant supply that bypasses the battery isolating switches. The radio memory takes a minimal current so can be equipped with the smallest capacity fuse that can be obtained. The bilge pump requires enough power to ensure that it can pump at full capacity when needed so must be protected by the correctly rated fuse as specified in the installation instructions. It is a simple matter to by-pass the isolating switch with a feed from the battery side of the switch into a dedicated always-on fuse-board.

Crimp terminals are our preferred method of joining cables as they provide a reliable and easily assembled connection method.



Crimp terminals come in a wide variety of types; far more than are shown here. This selection includes male and female blade connectors to suit various requirements, male and female bullet connectors, straight splice connectors, ring terminals again of various sizes and at the top of the display a piggy-back blade terminal designed to allow two cables to be connected onto one blade.

They come in three standard sizes to suit different cable diameters. Red is the smallest and yellow the biggest. Crimping tools vary from the cheap but perfectly adequate version shown here to heavy duty ratchet models that release when the correct pressure is applied.



The cutting function of the crimping tool provides for various cable sizes and makes a clean cut through the insulation without damaging the copper strands.

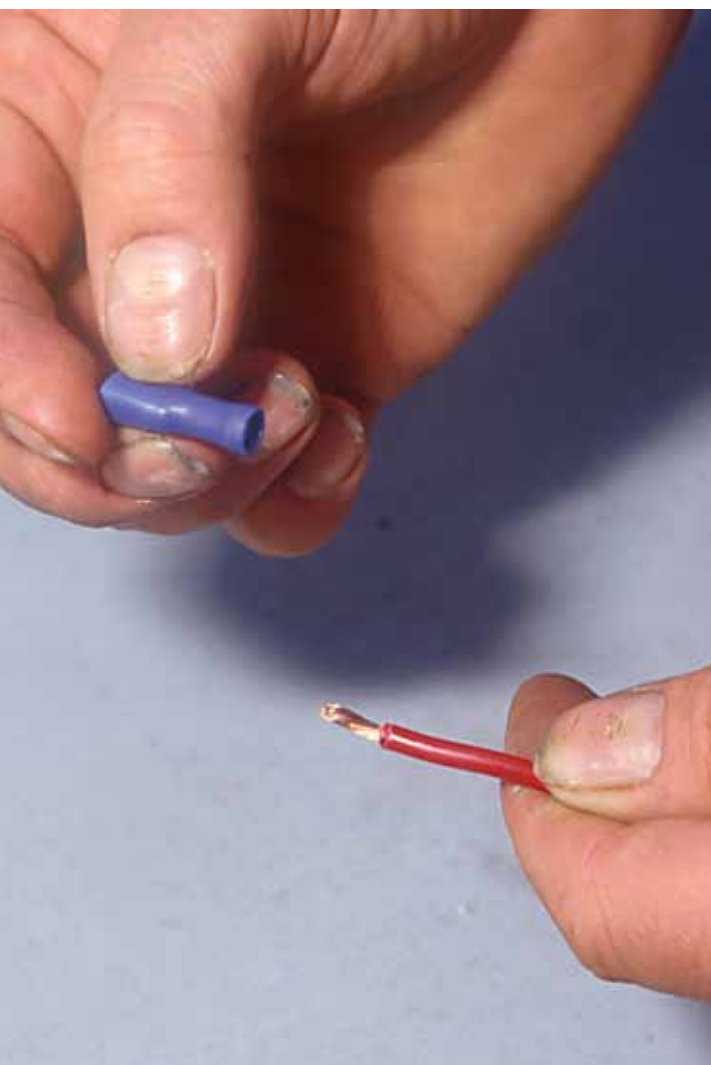


Once the cut is made the insulation is slipped off the core with the tool.

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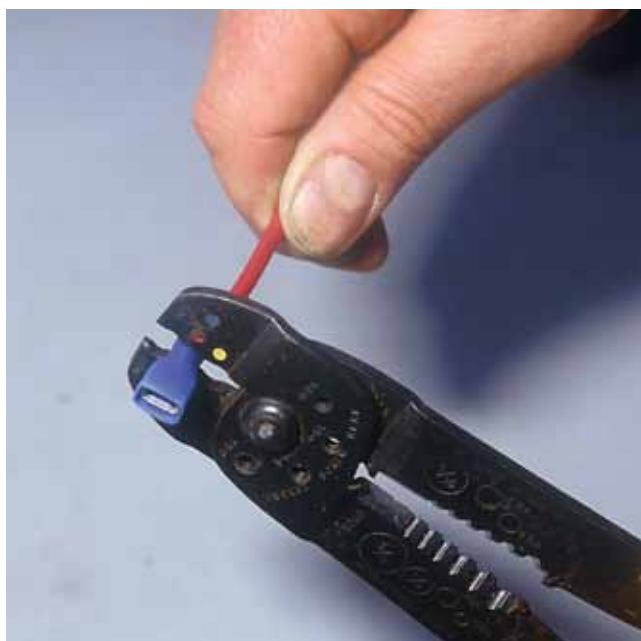


A terminal of the desired type and correct size for the cable is selected.

The terminal is slipped over the cable and pushed on until the insulation is felt to butt up inside. The correct colour coded compression section of the tool is then placed over the terminal and squeezed as tight as possible to 'crimp' the terminal onto the cable.

The finished joint is neat and not at all distorted.

Finally, always give the terminal a good hard tug to ensure it is properly crimped on. It should be almost



impossible to pull off a properly crimped terminal. The only time I have known a crimp to fail is when a large terminal is used on undersized cable.

Next issue we will be looking at the galley and gas system.

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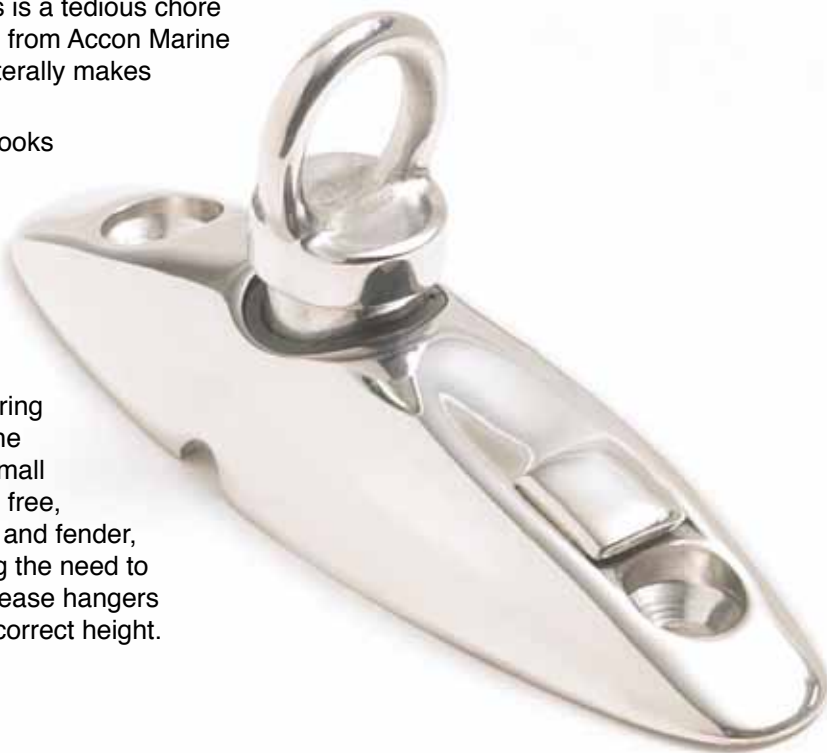
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BUILDING ROSIE LEE



PART FIVE: THE END OF THE JOURNEY



by **PAUL RODWELL**

From the beginning my wife Jane encouraged the project. It began in October 2014.



Jane admiring the model.

In both boating magazines I subscribe to was an article by Canadian Roy Schreyer about his house boat *Dianne's Rose*. (AABB #87) The look of it was attractive enough for me to read about a different sort of boat – I sail a modified Hartley 16 TS – but as I read the introduction I knew it was important! Roy designed *Dianne's Rose* for his wife Dianne. She couldn't handle Roy's sailing boat as it was 'tippy'. *Dianne's Rose's* main feature is her stability. Her second feature is ease of getting around in her – 6ft / 6in headroom!

My wife Jane had a similar aversion to tippy boats, she also found as time went on that small boat cabins become impossible to negotiate (I'm finding that approaching on me too!)

Jane read about *Dianne's Rose* and said something along the lines of "We could do that too".

So the journey began and I have recorded it in previous articles in AABB.

I sent to Roy for study plans then full plans. I wanted to make modifications to my houseboat to make it even more 'user friendly for less agile elderly boaters'. I discussed these with Roy and to get the ideas into 3D I made an eighth scale model (AABB #89)

Jane chose the name *Rosie Lee* right at the beginning. It put her boat in the same family as my *Missee Lee* but it also means, in English rhyming slang, 'cup of tea', hence the tea cup on the name panel and flag.

BRIEF HISTORY TO DATE

December 2014 saw the beginnings of the build and the barge-like hull was completed by May 5, 2015 (AABB #91). The cabin roof went on in early December 2015 (AABB #93).

The next part of the work, 'the fit out', is made up of lots of small things and seemed to go on for ever. In one article I read on home boatbuilding it said 25% for the hull, 25% for the cabin so the rest is 50%! In my case it has proved correct.

I have written of the fitting of the electric propulsion system in AABB #98 and about the rest of the 'fit out' in AABB #99. The bulk of that was done by mid August 2016 and a September launching looked likely. The only big thing left was the galley and shelving. Jane had persuaded me to have the cushions upholstered professionally and they were looking great. Unfortunately that was Jane's last input; on August 19 she took sick and passed away three days later!

The project she had encouraged from the beginning stopped for a while but five months later I decided it had to be finished. The last items of the interior fitout were done by mid March 2017 and *Rosie* was ready to leave her shed.

LEAVING HER SHED BEHIND

On March 23 Peter and I hauled *Rosie Lee* out of her shed.

To move *Rosie* I built a 'sledge' under her from two 4 x 2's covered in carpet which rested on 4 inch Kopperlogs as rollers. Then with a come-along winch we pulled her out into the yard.

In the shed she looked much bigger than outside (she looks even smaller on the lake!)

LAST MINUTE THINGS

It is amazing the little things that needed doing before *Rosie* would proceed to the water.

The major things were obvious – moving the solar panels from the shed to the boat and the final coats of blue paint.

Rosie hadn't been in rain before; now the windows would be tested and found to be excellent but

there are nasty drips if the front door is open! This was solved by a plywood canopy over the door that has a lip on it making a shallow gutter. Now you can open the door in the rain and not have to go through a waterfall.

I had always intended to have curtains and it is now that I finally set to and made them! In keeping with her name I used a couple of former duvet covers that were mostly decorated with roses. Then there were the name panels that I mentioned in the last article, it was now that they actually appeared together with their frames. Yes it is a *rosie lee* (cup of tea) in the centre. The same emblem is on the flag that was



ABOVE: On the trailer, ready to go.

INSET: *Rosie Lee* leaves her shed.

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Rosie Lee was 'officially' named with, as appropriate to her name, a cup of tea!

A few at a time the guests were taken for a trip round the inlet then we all had *rosie lee* (or coffee for some) and all sorts of goodies to eat.

'THE OUTBACK MARINA'

In the early days of the build I had arranged to keep *Rosie Lee* in the creek at the bottom of a friends property near Yungarburra. However, due to three poor wet seasons in a row, Lake Tinaroo is only about 44% full. One of the empty places is my prospective mooring! A number of other sailors have the same problem but at the other end of the lake there is still a sheltered gully with water in it. *Rosie Lee* is now there in company with a number of other craft, its as much like a 'marina' as I want! I can sit at my breakfast table and watch wallabies and water fowl and be at peace.

HOW DOES ROSIE LEE PERFORM?

She does everything Jane wanted and more!

You can walk straight on from the shore, sit on the benches of the foredeck or enter the cabin down two small steps. There you won't have to worry about the ceiling unless you are a giant and the berths are just the right height to be easy to sit on and get up from. Walking around is little different to the shore unless some wake from a ski boat comes along but even then the motion is no more than in a bus or train – so stable and so different from a round bottomed boat. It feels so different standing to cook at the stove and not have to be bent at all! And the toilet smells slightly of coffee!

It was these sorts of things that we planned from the start and which would have made boating a pleasure for Jane.



TOP: *Rosie Lee* as Jane last saw her.

ABOVE: Nice and snug inside.

BUT WHAT OF THE MOTOR AND ELECTRICS?

They too are better than expected.

I have done one longish trip 10-12km.

I checked the power consumption with the solar array turned off, just the draw from the batteries, and measured the speed with a hand held GPS. At the first notch (slowest speed) we did 4-4.5km/hr at 34amps; at top notch it was 6.3km at 62amp. 4.5km/hr seems quite fast enough to me!

Over the journey of about three hours I used 25% of the battery and by the following afternoon the battery was back to 100% and in lightly overcast conditions. So my aim of being able to go 12-15km, spend time in something like watching the wildlife or painting and return a day or so later will work just fine. Lots of short moves will be possible over several days.

SHE'S IN THE WATER BUT I STILL THINK OF THINGS TO DO!

The first night on the water I could 'see' a fold-down desk on the port side opposite the control area. That was quickly made and is used for all sorts. It's a good breakfast spot, a place to write or use as a chart table; it can fold down out of the way.

I have worked out how to change the motors' clamps so they can be screwed from the outside removing the contortion of turning them from inside the motor wells.

Things for the future – I really need a reading light at the head of the berth if I read in bed. On the topic of lights; all the cabin lights I bought have built in switches, with the cabin ceiling above the berth at 6 ft. you can't reach the switch without getting out of bed! Then it would be good to have some sort of fender rail across the stern, out board from the motors, to prevent damage to them or a vessel if one gets too close!

***So it goes on! Who would want it otherwise?
That is the next phase of the journey.***

A QUICK SUMMARY OF THE BUILD



I recorded time worked and the job done also all the expenses (but without looking at invoices I don't know quantities of materials)

TIME:

I took 1240 hours over two and a half years (six to nine months were taken out because of Jane and Ben's passing so it ought to be less than two years!)

The break down is:

270 hours to roll over (for the hull)	22%
460 hours for the cabin	37%
130 hours on motors and electrics	10%

COSTS:

The total cost was \$13,100, more than I had ever envisioned but as it was mostly monthly bills at Ravenshoe Hardware it wasn't too noticeable over two years!

MAJOR ITEMS WERE:

Timber and plywood	\$2160	16.5%
Glues and paints	\$3271 (half of that was epoxy)	25%
Acrylic for windows	\$400	3%
The Electric System	\$4400	34%

I saved a considerable amount on the timber by using recycled boards

MAJOR SUPPLIERS WERE:

Ravenshoe Hardware
All4solar
Cairns Coatings
Nuplex, Cairns
Whitworths, Cairns
Haymans, Atherton

For almost everything not listed below, but some of that too!
The electrical system
Paint and glues
Epoxy, glass fibre cloth
Numerous items including epoxy, paint, LED lights
Electric wiring



Norbert (left) with proud builders and the Glühwürmchen, ready for sailing in Berlin.



'BUGNEWS

by **PETER TAIT**

MULTIPLE FAMILIES BUILD A 'BUG IN BERLIN!'



ABOVE LEFT: Katinka, Merta and Ida keen for a sail!!



LEFT: Holger enjoying a sail on the lake.

ABOVE: Interesting artwork! The builder's hand stamps on white then the stencilled name over the top. Nice job. The name 'Glühwürmchen' is a direct translation of the word FireBug or Firefly.

Norbert Pauli in Berlin writes *"In a team effort we built, launched and finally sailed Glühwürmchen last week at Lake Parstein, Berlin, Germany. The builders were six children and six adults working Saturdays over a period of 18 months, all sawing, planing, and glueing framing lumber and plywood. Many times the Saturday building hours resulted in group lunches –*

special thanks to those who supplied the food! During this summer holiday we saw the last pieces coming together and put the boat to water. Favourable weather, good food, and nearly all the builders together for the first sail. It was a great day! Hosting this endeavour for me has been very exciting. Please find pictures attached. We would be proud to find one of them in the FireBug newsletter. Thank you all so much!" – Norbert and the Glühwürmchen team.

COMPETITION! WIN A NEW SAIL!

Bill Barry, longtime FireBug supporter and sailmaker has developed a new Mk 5 sail. He's made one, it looks beaut and it's the competition prize! How you enter. Write 3-400 words on **'What makes my FireBug so Special!'** Judging will take age into consideration. Email your entry to pete@firebug.co.nz and include a photo of yourself and the boat, partly built is okay.



FUNDING AT CONCORD RYDE

Ross Venner from Concord Ryde SC reports that the club, working with the Australian Sports Foundation, has raised \$10,000 to allow pupils attending local high schools to learn to sail and then participate in a 'Bug Build. While CRSC is close to Meadowbank TAFE, home of Sydney's only boat building school, there is no assumption that participants will seek one of the 30,000 jobs in the industry. A young person who can say, "I sail and I built a boat" will surely stand out among candidates for almost any job. To quote one businessman, "I can't get stickability like that anywhere."

FIREBUGS AT THE AWBF IN HOBART



Ben Glover sailing *Hot Buggy* at the AWBF.

Once again there were 'Bugs sailing at the fabo Australian Wooden Boat Festival. Rod Stevens advises from Hobart that the Lindisfarne Sailing Club has a regular fleet of five boats racing and there are more boats available to buy or borrow – how about some adults using those spare boats to boost the fleet numbers and provide a challenge for the kids. They love a chance to beat the dads or sailing instructors!

**REAL SAILORS BUILD
THEIR OWN BOATS!**

**REAL LIFE SKILLS –
BUILD A YACHT!**

SOME NEW LAUNCHINGS

Hi Pete, I had success a few weeks ago, launching in a light breeze, what has been named The Grinch, (main character from the Dr Seuss children's story). All went well. The boat is not painted green, it is adhesive vinyl that can be removed and the colour changed. Good trick!" Regards Aaron Dixon, Brisbane.

Recent launching of two boats in Tauranga, New Zealand by Philip Churchill. A smart looking boat each for sons Timothy, 12 and Michael, 9. Phil two pot painted the spars white and they look great! The boats are shown being launched from the front lawn, so lucky!



The Grinch.



Churchill boats in Tauranga.

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FIREBUGS IN OREGON. (NOTHING TO DO WITH THE RECENT BUSHFIRES!)

Brandt Eilers in Portland Oregon USA writes *"Peter, Here are some build photos and finished pics, (see below) We named the boat Flying Squirrel and launched it on September 2. The build took part of two summers since I had to build on my front porch. The sealed hull sat under a tarp all winter waiting for warm weather to put the paint and varnish on. Then I painted the hull to match the first boat I built which is a 10' stitch and glue skiff. Overall the build wasn't too much more complex, the hardest part for me was learning to use a metric tape measure. The boat sails very well, and is a blast in moderate wind. My nine year old son and I took it out yesterday in our local*



Brandt's *Flying Squirrel* under construction.
The boat has been launched.

river and had a great time, even with strong gusts the boat stood up well, with just a little chop splashing over. It's nice to know that even if it does go over, it will come up fairly dry and ready to go. I'm thinking it will be a great sail training boat for my son (when I let him use it!). I love the way the boat turned out and I'm hoping for a few more good sailing days this year before the weather turns. The few days I've been able to get out this fall were so fun, I'm considering a drysuit for winter sailing. If not, I'll be eagerly waiting for next spring. Thanks for the great plans, beautiful sail, and support with the wooden spars and hardware. I will be spreading the word about what a wonderful unique boat the Firebug is. Thanks and happy sailing, Brandt.

WHAT THE 'BUG'S ABOUT

The FireBug has always primarily been about; *Learning life skills for kids: reading plans, measuring, basic maths, cutting and shaping, choosing and buying, gluing and painting, graphics, names, boats and sailing.*

As a senior educator in Australia said: *"It's a great idea, kids, especially boys would enjoy learning for a change but to get it into the system is like trying to change the direction of a dinosaur!"*

It reminds me of the 18 year old who was offered a job in the movies. His parents sprang him out of University and although they were looking for someone much older he passed the months trial and got a fantastic job. Later he told his father: *Dad, if I hadn't been able to build and sail boats I wouldn't have got that job!"*

No study, no student loan, fantastic career learnt on the job.

SMALL SAIL BOAT FOILING



Our spy camera spotted this boat doing secret trials on Wellington harbour by night! It's an Opti and are they doing secret training for the World Champs? Has anyone tried 'BugFoiling? If you're keen contact me with your ideas. pete@firebug.co.nz And as Norbert asks *"What about ice sailing in a 'Bug?"*

NEW 7 BOAT FLEET IN GORE



Peter Salmond writes from the Gore Boat Club in Southland NZ "Peter, I've attached a photo of three of our fleet of seven FireBugs at the Southland Boat Show. We sail on a 40 acre lake which is ideal for the FireBugs and have purchased, through grants, a safety boat which is proving invaluable. The kids love the boats!"

BRETT AND HENRY IN GRENVILLE



Brett Purssell from Grenfell, west of Sydney (home of famous Aussie poet Henry Lawson and hence the name) has almost completed *Henry* and is looking

forward to get afloat. Brett's family are experienced boat builders and it shows, lovely job! He says "Pete, it's been great fun. I got the sail, WOW fantastic! What a beauty, great colours, I won't get lost on water!"

NEW SAILOR ON THE WAIMAKARIRI RIVER



Kerry O'Brien's daughter Stalena enjoying a sail in the recently launched *Dawn Treader*. Kerry is the commodore of the Waimakariri Sailing Club, based on the Waimakariri River near Christchurch. It looks like a beaut place to sail!

NEW FIREBUG WEBSITE

The new website is on the 'back burner' awaiting funding. Hopefully some will turn up soon!

All enquiries: pete@firebug.co.nz

REGATTA DATES FOR THE SOUTH ISLAND

Tom Arthur advises regatta dates:

South Island Champs

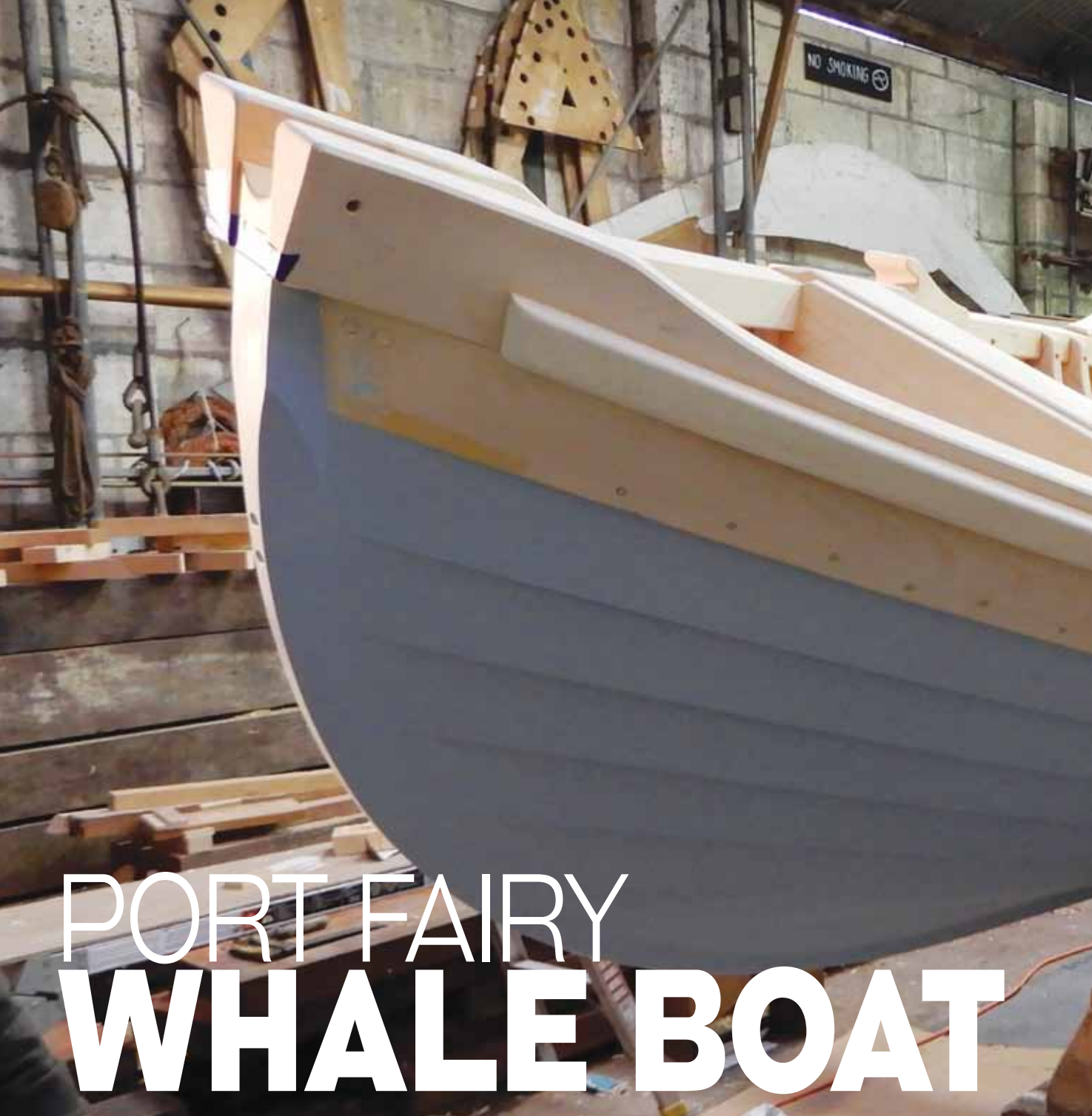
at PPYC December 9.

Canterbury Champs

at the Waimakariri Sailing Club,
March 10-11, 2018.

Lake Mahinapua Aquatic Club Regatta
in Westland February 3-4, 2018. Camp by
the lake! Further info: tom.arthur@orcon.net.nz

****Aussie entrants welcome, borrow a boat!**



PORT FAIRY WHALE BOAT

by **JONATHAN WALLIS**

A recent trip to renew some mainland maritime, and in particular Huon pine, links with Tasmania took us to South Australia via Victoria's Western coastline.



After an initial shock at the density of tourist traffic from Melbourne along the much-anticipated Great Ocean Road and Ship Wreck Coast, fortunately, after Port Campbell the traffic eased giving us an easy run to Warrnambool. Here we were able to meet up with the dynamic Patrick and Susie Groot and catch up with the news of forthcoming regattas and Saint Ayles skiff events in the region. Happily, Patrick gave us an introduction to Garry Stewart and the other good people from the active Maritime History group of enthusiasts in Port Fairy.

Port Fairy was named in approximately 1810 when Captain James Wishart dropped anchor there on board the whaling vessel *Fairy* and navigated the Moyne River there and named the port after his vessel.

Subsequently in around 1835 James Wishart established a whaling station on Griffiths Island at the mouth of the Moyne River from whence the whalers embarked in their sturdy vessels, often using similar designs to those of the well-known builder 'Beetle' from North Connecticut, and chased down the then plentiful migratory whales from the nearby beach.



Port Fairy SR lifeboat. Image Jonathan Wallis

The port is open to the ravages of Bass Strait but is today a sheltered riverside harbour providing haven to all manner of vessels from pleasure craft to working fishing boats and commercial vessels. We were once again impressed with the maritime atmosphere in this picturesque town and surrounds, including the breakwater and historic lighthouse on nearby Griffiths Island.

RACKS OF TRADITIONAL TOOLS, AND ALL THE ACCOUTREMENTS OF A TRUE CRAFTSMAN, AN OCCASIONAL WOODEN DINGHY, EVEN A BEAUTIFUL NEWLY COMPLETED LAUNCH, ITS HULL GLEAMING WITH A GLASSY FINISH, ALMOST PROVIDE AN OVERLOAD FOR THE SENSES!

The port also has an unbroken tradition of boat building especially of traditional vessels, often wooden. So, we were delighted to meet the very talented Garry Stewart who is a traditional craftsman there, who was engaged in the construction of an

eye-catching American 'Beetle' type design whale boat in his riverside yard at 39 Gipps Street.

Garry Stewart describes himself as "specialising in custom built traditional timber boats of high quality workmanship" and indeed to step inside his commodious shed is to walk from the 21st Century into an environment redolent of a more gracious era.

The traditional tang of freshly planed wood, in this case Huon pine, immediately resets the senses. Racks of traditional tools, and all the accoutrements of a true craftsman, an occasional wooden dinghy, even a beautiful newly completed launch, its hull gleaming with a glassy finish, almost provide an overload for the senses!

With a length of 28' 9" she has a beam of 6' 6" and drawing 10 to 12" the whale boat is a

tough multi-purpose vessel robust enough to chase down a whale and tow it back to the beach, yet ideal for rowing and making a good speed under adverse seas and conditions. In fair weather it can additionally be sailed with a following wind.



The vessel also has to be roomy enough to allow the crew of five rowers, one of them the harpooner for'd, and the steersman aft, to row and sail the vessel, generally work and not get in each other's way, or to interfere with the harpooner, or his line as it snakes forward from the drum next to the red gum 'logger head' aft.

The vessel is constructed almost entirely from Huon Pine sourced from Tasmania and is of traditional 'Lapstrake' (Clinker) hull construction, fitted out as it would have been used at Port Fairy in the 1840's. Garry told us: "I think Huon Pine is a great timber for boat building, with its durability, little movement in service, its good workability, plus it even smells nice." He added: "I will be starting a set of oars and a sweep oar for the whaleboat shortly." The only exceptions to

ABOVE LEFT: Macrocarpa knees.
Image Jonathan Wallis

ABOVE RIGHT: Red gum logger head.
Image Jonathan Wallis



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Huon Pine in the build are the *Macrocarpa* pine thwart knees, and the 'logger head' aft.

The whale boat has been completed now and has been taken to another shed where the group are completing the oiling subsequent to launching and naming it.

Garry completed his apprenticeship with his father, Ron Stewart, in Port Fairy, early acquiring there a full appreciation of the aesthetics of working with the best timbers and resources, and in particular the beautiful water resistant, strong and light pine from Tasmania. This in fact has proved an enduring love for Garry and his timber of choice to this day.

Today Garry's yard is kept busy with maintenance and Survey work and serving the needs of the marine community in Port Fairy when he is not engaged on building major vessels; he is also about to launch a new boat and is, as always busy.

There are several other whale boats in the area including the *Sir Fletcher Jones* stationed at Port Fairy and two sister vessels, all built by Garry in 1988, plus another similar, at Flagstaff Hill in Warrnambool.

There has been a long-established tradition of racing them annually and recently, against stiff opposition, it was a whale boat, *City of Warrnambool* that won the hotly contested '2017 South West Regatta', against even the beautiful Ian Oughtred designed Saint Ayles skiffs.

The 2017 regatta also saw the welcome rebirth of the 'Australian Whale Boat Championships' which had been held there in previous years from 1988 but which languished for a few years until 2007, when the boats were reconditioned and it ran until 2016, and is thankfully now revived.

We were subsequently shown over the very impressive 1857 built wooden vessel *Port Fairy*, restored by Garry Stewart in 1997, the oldest operational self-righting lifeboat in Australia. The lifeboat



LEFT FROM TOP:

Garry Stewart himself.
Image Jonathan Wallis

Garry's latest launch. Image Garry Stewart

For'd section plus mast step.
Image Jonathan Wallis

is maintained in authentic and original condition, and though a heavy vessel to row, it is still taken out regularly from its purpose built shed on the river. Much more can be written about this vessel perhaps in a future edition, however suffice it to say that if you are a wooden or historical boat enthusiast, Port Fairy should be on your list of essential destinations. And no visit would be complete without looking in on Garry Stewart and his premises there.



ABOVE RIGHT: Port Fairy plus Sir Fletcher Jones. Image Jonathan Wallis

RIGHT: Garry's 1984 Fishing boat *Ultimate Ambition*. Image Garry Stewart



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SYDNEY'S CLASSIC & WOODEN BOAT FESTIVAL



So what is that What's-A
MaCallit?

by **DAVE GIDDINGS**

It's back, Sydney's Classic & Wooden Boat Festival and it caters for more than wooden boats. Classic and wooden boat owners and boating enthusiasts alike; this will be your biggest weekend of the year! The festival is free to the general public.

April 2018 will see many important events fighting for prominence on your calendar and The Sydney Classic & Wooden Boat Festival (CWBFF) should be underlined and in bold! The festival of the boatie year is returning to the Australian National Maritime Museum (ANMM) from Friday, to Sunday, April 13-15, 2018. The atmosphere will be colourful from the marina in Cockle Bay full of boats to model boats scooting around in the basin. Boats on trailers will be placed from the Pyrmont Bridge, sprawling all the way along both sides of the museum

precinct down to wharf 7. The water, footpath and air will be alive with the hubbub of boatie's scuttlebutt and pride.

The organising committee's goals for 2018 include, having over 150 vessels, great and small, afloat and ashore for your viewing pleasure, to have many a demonstration to view, harbour cruises, children's activities and plenty of opportunities for young and not-so-young to be involved! With over 15, 000 visitors in 2016 the organiser's 'ashore' you, 2018 will be a festival not to be missed.



Children having fun onboard.

The theme for the 2016 festival was a celebration of the 'best of Australia's maritime and boating heritage'. Plans are well under way to make 2018 bigger and better. Each festival has a different theme aiming towards the culmination of a six-year journey to 2020 to mark 250 years since Captain James Cook's arrival in Australia.



Boats abreast showing their colours.

This is being aided through input and contribution from the Wooden Boat Association of NSW, boating associations including the Halvo's, Historical Skiffies, Steam Boat Association of Australia, Sydney Heritage Fleet and major harbour sailing clubs. There is clearly a lot to get excited about as the three-day celebration of vintage, veteran and wooden boats reflects the time 'when boats were made of wood and the men were made of steel'.

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PROVIDE A PRETTY REALISTIC
RECOLLECTION OF **LIFE AT SEA**
ON A WARRIE THAT WILL SEND A
SHIVER DOWN YOUR SPINE

Maybe, we should mention steel boats also, as many great examples are present on Sydney Harbour. One of these is the *James Craig* which is one of few iron square rigged ships still operational in the world. I have good scuttlebutt that she will be doing cruises on the harbour over the weekend along with the Sydney Heritage Fleets *Lady Hopetoun*. This will be a great opportunity to spend some time around steam to refresh the memory for all of you mature enough to have spent time steaming real ships.

There is also the museum's collection of old warries including the O-Boat Ex *HMAS Onslow*, Ex *HMAS Vampire* and the patrol boat Ex *HMAS Advance*. For all of you old matelot's, the Action Stations Exhibition and theatrette provide a pretty realistic recollection of life at sea on a warrie that will send a shiver down your spine.

As we saw in previous CWBF's there will be a large fleet of Halvorsen's from 25-60ft bobbing alongside examples of some of the best wooden and early fibreglass boats ever built in Australia including my Clansman. With a rise in amateur and professional boat builders restoring and building with the finesse of



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James Craig under way on Sydney Harbour.

a bygone era there will be something for everyone. There are a growing number of traders who will be displaying their wares. For example the team from DRIVE Marine and BoatCraft Pacific will be there showing off the advantages of Bote Cote and the other products you need to build repair or restore your boat and why they are 'a Safer Way to Work'.

BRING YOUR LAUNCH OR YACHT AROUND TO MAKE A WEEKEND OF IT WITH LIKEMINDED PEOPLE WHO LOVE VINTAGE AND CLASSIC BOATS



Classics @ CWBF 2016 – action stations in background.

If history wets your whistle, the fleet of restored Army Workboats (AWB) with some 300 plus built during the Second World War will be a sight to see. Then there is the *Ena* which is a masterpiece with a long history and now being pampered at the ANMM. Plus, of course, the more the merrier as we would love you to bring your launch or yacht around to make a weekend of it with likeminded people who love vintage and classic boats and the infectious enthusiasm towards restored boats from yesteryear and masterpieces by those who love to build boats from wood. Expressions of Interest closed on November 17, 2017. Although, you are still welcome to submit an application after that date to exhibit, but you may find yourself on a waiting list if all posies are taken up. To make an application for the museum or Cockle Bay marina all relevant information and Expressions of Interest Forms are located at www.anmm.gov.au/cwbf

Planning is well under way for a Quick-N-Dirty Boat Building Competition. Are you a whizz at designing and building weird and wonderful boats? Or have you always wanted to build a boat in two and a half hours? Then, the festival wants you and your mates to spare some time on the Saturday to hatch a master plan.



Some fine examples of restored yachts and cruisers.

If this sounds like you, the Q-N-D Competition is the place to be. Front up on the Saturday with your design / ideas ready for a hectic two and a half hours. DRIVE Marine Services and BoatCraft Pacific are sponsoring the event and you will be supplied with all the materials, including two sheets of plywood and doctored EPOX-E-Glue to hold together your masterpiece. Come back on Sunday to add some flare with a splash of colour and design before jumping in the water Sunday afternoon ready for the test of durability with some creative cheating while manoeuvring around a course to become the Q-N-D champion.

So, if you think you have some mates who could pull off the prize nominate your team. Teams are limited to a maximum of six due to space, so get your application in now, at www.anmm.gov.au/cwbf



A creative Q-N-D from Goolwa 2017.



Q-N-D action on the water.

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LEFT: Novice canoe builders at C&WBF 2016.

BELOW LEFT: Model of RAN Standard launch.



happen is Alesha Bleakley – the dynamo teacher who is charged with digging up the teams and looking after the myriad of paperwork required to get students out of the classroom. There are a couple of videos on YouTube under 'Bellinger Canoe' which show the students having a ball pulling a canoe into shape. It is amazing to see green students working cooperatively at a swift pace bringing the canoes to life. Close of day one, will see all teams with canoes pulled into shape using cable ties and ready to have a 'gooping' good time with filleting all of the seams. By Saturday evening all seams will be filleted and a coat of Bote-Cote applied on the inside.

If you know a high school that may be interested in entering a team in the canoe

The 2016 Sydney CWBF saw the first Novice Canoe Building Challenge by teams of high school students. Given the great success in the first round, the challenge is set to go ahead again! In teams, the students will build a Bellinger Canoe over three days. Before, launching head first, into Darling Harbour to race their canoe around Cockle Bay to test their teacher's dexterity.

We hear you ask – "How can students with no prior knowledge build a canoe ready for the water in three days?" Well, this is made possible with the expert guidance of Brian Jones (the designer of the Bellinger and retired primary school teacher) using materials donated by DRIVE Marine and BoatCraft Pacific. Pulling together pre-cut panels of 4mm plywood is made simple through Brian's expert guidance and the use of cable ties along with Bote-Cote Epoxy Resin thickened with Bote-Cote's easy to mix Gluing and Filleting Filler. The other key player in making it

build or you are interested in entering the Quick-N-Dirty Boat Building Competition contact the ANMM at <http://www.anmm.gov.au/whats-on/calendar/classic-wooden-boat-festival> or the C&WBF Project Manager at 02 9298 3777.

These are a few of the activities that are being planned to occupy the time of those young and old over the weekend. There will also be model boat displays, marine artworks and many other activities for you to ensure you need a couple of days to take it all in.

See you at the Australian National Maritime Museum April 13-15, 2018. Put it in your diary – Oops, Outlook calendar or whatever Fangle Dangle technology you use to jolt your memory.

COPPERCOAT

COPPERCOAT AFTER 27 YEARS!



We were delighted to recently hear from the owner of one of the very first boats to be painted with Coppercoat. George kindly informed us that the coating is still performing, 27 years on!

George tells us about the latest lift-out: "During this October 2017 lift-out for maintenance after two years of being afloat, it can be seen that after hosing, the hull requires minimal maintenance, just the boot topping, and touching up the odd scratch on the Coppercoated hull."

Prior to being purchased by the current owner, this Cromarty 36 spent most of her life in sea water. She is now used mainly in a freshwater bay in Cardiff and makes regular Bristol Channel sea water passages during the summer season.

This Cromarty 36 motorsailer built in 1989 was Coppercoated from new and is an incredible example of how a proper treatment of Coppercoat can provide long-lasting and cost effective protection. To find out more about how Coppercoat works, please get in touch <http://coppercoat.com/contact/>

CLIPPER RACE RECORD BREAKER



With The Clipper Round the World Yacht Race yachts coated for the first time with Coppercoat antifouling, the competing yachts have started setting new Clipper Race records. Stage two winner *Greenings* has covered the most nautical miles in a 24hr period, after notching up 329nm on day nine.

To achieve the consistent high speeds required to cover great 24-hour distances, the hull must be super clean and hydro-dynamically efficient. Coppercoat has been shown to prevent the kind of fouling build up that can plague yachts sailing in the mid-latitudes. The entire Clipper 2017-18 race fleet has been Coppercoated which will not only protect the yachts for longer but also provide the smooth surface required to maintain sailing efficiency throughout the race.

Coppercoat Director Ewan Clark commented: "*We would like to congratulate the Greenings crew for an excellent job in breaking the 24-hour distance record for the Clipper Race. This is the first time the yachts have raced with Coppercoat hull protection and we are delighted that the performance of our product is meeting expectations. I am sure this will not be the last record of this exciting and challenging race.*"

Unlike traditional antifoul paints, Coppercoat is made from a combination of solvent-free epoxy resin and high purity (99%) recycled copper, making it the strongest copper based antifouling available. Coppercoat exposes active copper to protect the yacht's surface from any fouling organisms that the Clipper Race fleet will endure during the race around the globe. In addition, the smooth surface provided by Coppercoat will deliver less drag from its long lasting clean hull, which will achieve better performance during the 11 month competition.





THE **YANMAR WOODEN BOAT SHOP** WOODEN BOAT FESTIVAL OF GEELONG



Come down to see our great fleet of Classic Wooden Dinghies as they battle for line honours.

images **TOM SMEATON PHOTOGRAPHY**

The 2018 Wooden Boat festival of Geelong will be held over the Labour Day weekend commencing on Saturday, March 10 and running until Monday 12. This will be the eighth festival and it will again attract the highest calibre of boats from all around Victoria and interstate.

We are expecting over 175 entries, approximately 1200 direct participants and in excess of 30,000 visitors to the waterfront festival, there is no doubt that the event is growing and it has become a major biannual regatta on the sailing calendar.

The festival gives the Geelong community the opportunity to see first-hand some of the most historic wooden boats in Australia.

Three Tall Ships have confirmed their attendance – the *Enterprize*, *Alexander Stewart* and the *Julie Burgess* from Tasmania. These boats will be taking visitors on regular trips around the bay as well as hosting other events including whisky tastings.

THREE TALL SHIPS
HAVE CONFIRMED
THEIR ATTENDANCE
– THE **ENTERPRIZE**,
ALEXANDER
STEWART AND THE
JULIE BURGESS
FROM TASMANIA

Located an hour from Melbourne's CBD, Geelong is recognised as Victoria's largest regional city. The region's natural beauty, state-of-the-art infrastructure and the newly renovated and rejuvenated waterfront precinct makes Geelong the perfect city for events of all kinds. The weekend will feature loads of family fun. There will be treasure hunts, face

painting, an art competition and hands on Bote-Cote Quick and Dirty boat building competition presented by local community service organisations. Captain Jack Sparrow and the Funfit team will be on hand entertaining the kids throughout the weekend.



There's nothing like a gaffer to recreate the romance of sailing in a wooden boat.



Get ready to watch the Classics carve up Corio Bay as they contest for The Cup.



Friday night entertainment kicks off with our outdoor movie night and there will live music throughout the weekend as well as a busker's festival. The RAN Jazz band will be performing as well as a huge Rock and Roll night on Saturday.

Our extensive program which will comprise both on water and shore based activities, including a Grand Parade of Boats and Cavalcade of Sail, a Concourse d' Elegance, racing for the Corio Bay Couta Boat Cup, the Corio Bay Classic Wooden Yacht Cup and the Passage Races from Portarlington. We will again be running off the beach races. We are looking forward to a good fleet of old classic dinghies. The Victorian International Cadet State titles will also be held during the festival.

We are extremely pleased to have Mr Ian Smith as our keynote speaker. Ian is president of the Historical Sailing Skiff Association in Sydney and is the author of the book *The Open Boat – The 18 Footer, Origin, Evolution and Construction*. He is also a boat builder and regular sailor on Sydney Harbour with the Sydney Flying Squadron as well as being the moderator of the website www.openboat.com.au

**COME ALONG AND BRING YOUR FAMILY,
THERE'S PLENTY TO DO AND SEE.**

ABOVE: You will be able to see the majesty all our wooden beauties in the Sail Past.

INSET: On Saturday night of The Festival the sailors take to the floor for our Rock'N'Roll dance off.



All enquiries can be made by contacting Royal Geelong Yacht Club.

For more information call **03 5229 3705**

email info@wbfgelongs.com.au

or visit the website

www.woodenboatfestivalgeelong.com.au

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MARCH
SAT 10th
Sun 11th
Mon 12th
2018

**KEY NOTE
SPEAKER**
Ian Smith,
president of the
Australian
Historical Skiff
Association



Royal Geelong Yacht Club
25 Eastern Beach Road
Geelong VIC 3220

Ph: 03 5229 3705

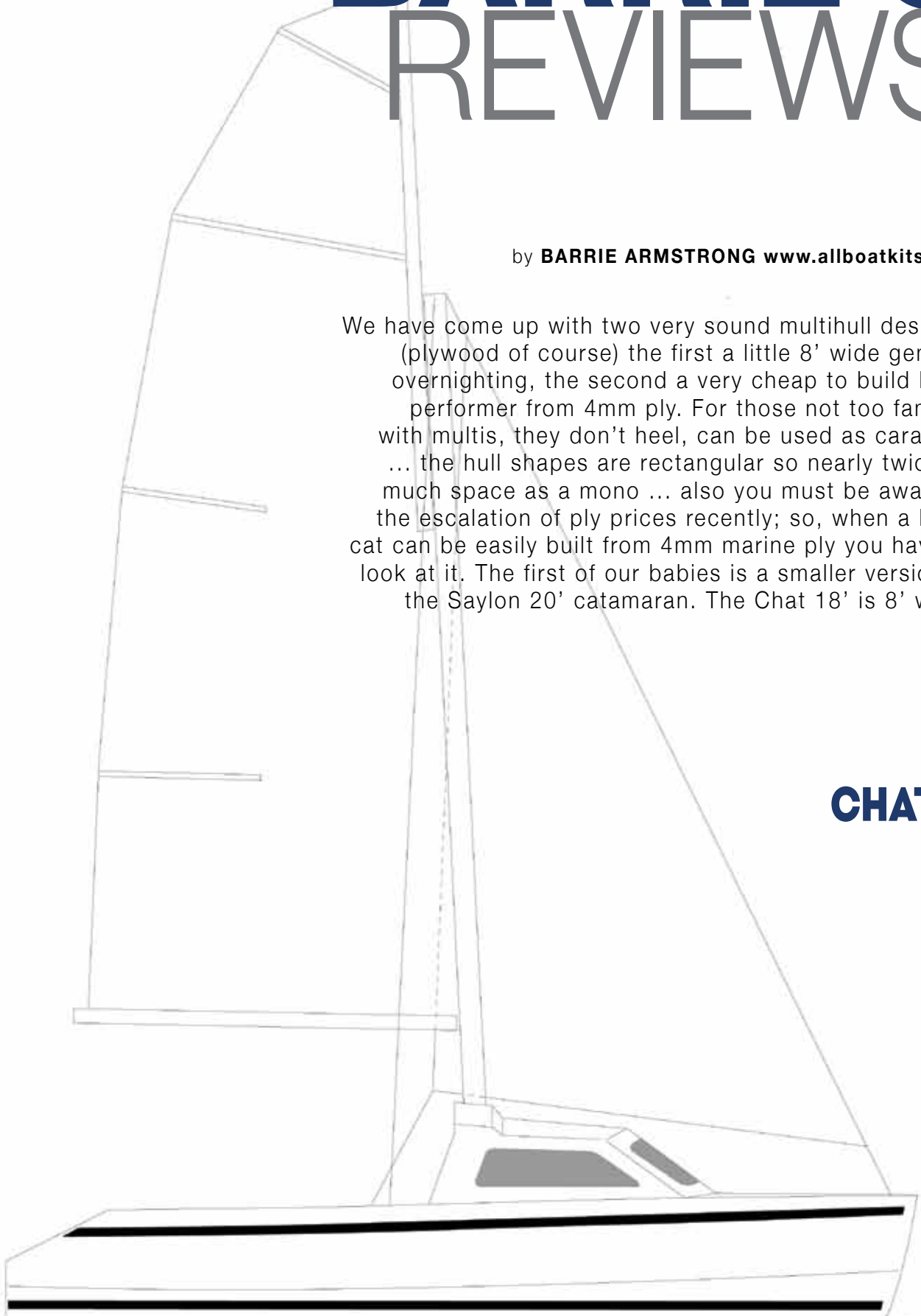
www.wbfgeelong.com.au

BARRIE'S REVIEWS

by **BARRIE ARMSTRONG** www.allboatkits.com

We have come up with two very sound multihull designs, (plywood of course) the first a little 8' wide gem for overnighting, the second a very cheap to build large performer from 4mm ply. For those not too familiar with multis, they don't heel, can be used as caravans ... the hull shapes are rectangular so nearly twice as much space as a mono ... also you must be aware of the escalation of ply prices recently; so, when a large cat can be easily built from 4mm marine ply you have to look at it. The first of our babies is a smaller version of the Saylon 20' catamaran. The Chat 18' is 8' wide.

CHAT18

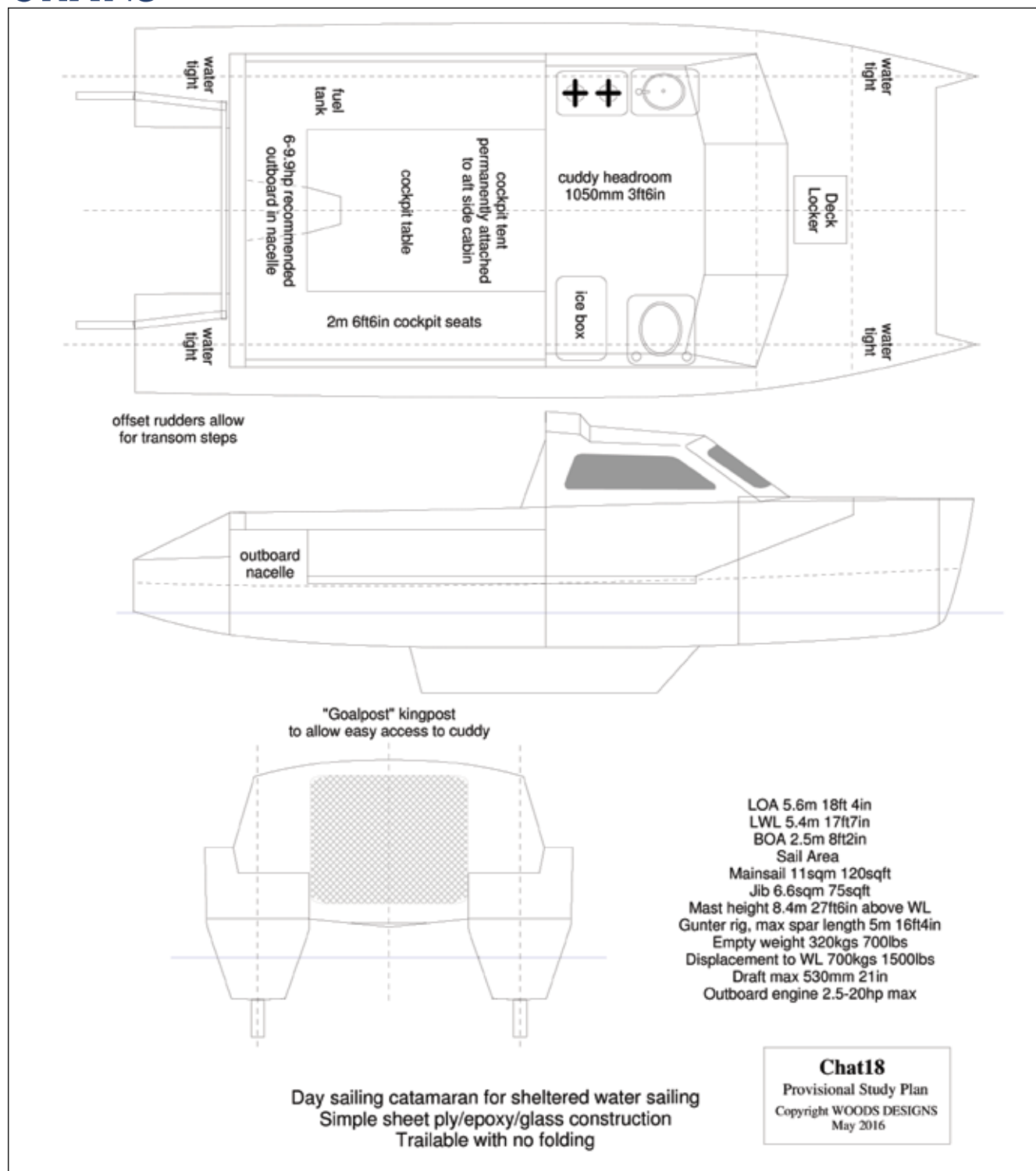


It is easy to trail with no assembly needed. Indeed it will be quicker than most small monohulls to get from trailer to water, typically 15 minutes. That's because raising the mast is quick and easy, there is no keel to lower. Furthermore, the boat is a rectangular platform, safe and easy to walk around on when onshore while most of the gear and the sails can be left in place when trailering. The trailer itself is

just a regular flat bed trailer, no supports or rollers are needed.

The small cuddy offers enough space for simple cooking, with icebox stowage, and privacy for a porta potti and sink. The cockpit seats are 2m (6ft6in) long so a tailor- made tent means it is quite feasible for two people to camp on board or use the Chat 18 as a caravan.

CHAT18



The rig features a gunter mast so it can be easily home built in wood using the standard 'birds mouth' construction. The gunter rig also keeps the spars short to simplify trailing and rigging. Of course, a conventional one piece mast can also be used, maybe an old beach cat rig?

The two full length battens increase both mainsail area and efficiency and allow the leech to twist off in wind gusts, while the 'soft' lower part is easier to 'read' and reduces rig weight and cost compared to a full battened sail.

EVERY PLANS PURCHASE ENTITLES THE OWNER-BUILDER TO SOME **GREAT DISCOUNTS ON EPOXY, PLY, FIBREGLASS, FITTING AND RIGGING, SAILS, MASTS ETC** FROM 'ALLBOATKITS'

The engine fits in a nacelle for easy access and to increase manouverability. A 2.5hp outboard (or maybe a yuloh) is suggested for sailing, but up to 20hp is possible when using the Chat 18 as a motor sailer with speeds around 10kts.

The Chat 18 is fitted with low aspect ratio keels for simplicity of both building and sailing, however daggerboards are an option for very shallow water sailing. The rudders are offset to increase the width of the transom boarding steps.

The high freeboard, plus the protection that the cuddy provides, helps keep crew and passengers dry. The

hulls have extra buoyancy aft to support crew weight (many small catamarans trim by the stern with crew on board). The bridgedeck clearance is kept as high as possible and it is 'Veed for extra stiffness and to reduce wave slap.

Despite its small size and the essential watertight compartments, there is still lots of storage space in the hulls for anchor, warps, fenders, deck cushions, , fishing tackle and all the other 'stuff' that makes a day out on the water more enjoyable.

The Chat 18 is very simple to build, featuring all flat panel plywood with glass/epoxy chine joints so there is no timber bevelling. It is small enough to build in a large carport using 'hardware store' materials and minimal tools, thus making it an ideal first boat project.

Current price of plans in Australia is \$210, downloaded electronically. (As soon as the money is in Paypal account your plans will be sent to you.) We hope you are pretty excited, because this new

version will work, and if 8,800 hits already mean anything, we are on a winner!

Part also of the plans deal is when you download your plans you will also receive an "extras" voucher.

DO NOT LOSE THIS ... every plans purchase entitles the owner-builder to some great discounts on epoxy, ply, fibreglass, fitting and rigging, sails, masts etc from 'allboatkits'

Of course, the Chat 18 can be used as a power cruiser or a motor sailer, a 20hp outboard gives 10kts in a very safe and comfortable manner.

This is a winning design, and I strongly believe very close to five out of five rating. Please let me know what you think by phone or email, and give me some feedback with your ideas. Check out also the solid cockpit floor for kids, and semi-permanent cockpit tent.

NEXT

Hold onto your hats, slowly open your eyes, and look at this ... It's a beauty!

Introducing another new design, brand new 21' stitch and tape, two berth trailable catamaran!

BASIC MATERIALS LIST (APPROX, NO ALLOWANCE FOR WASTE OR ERRORS)

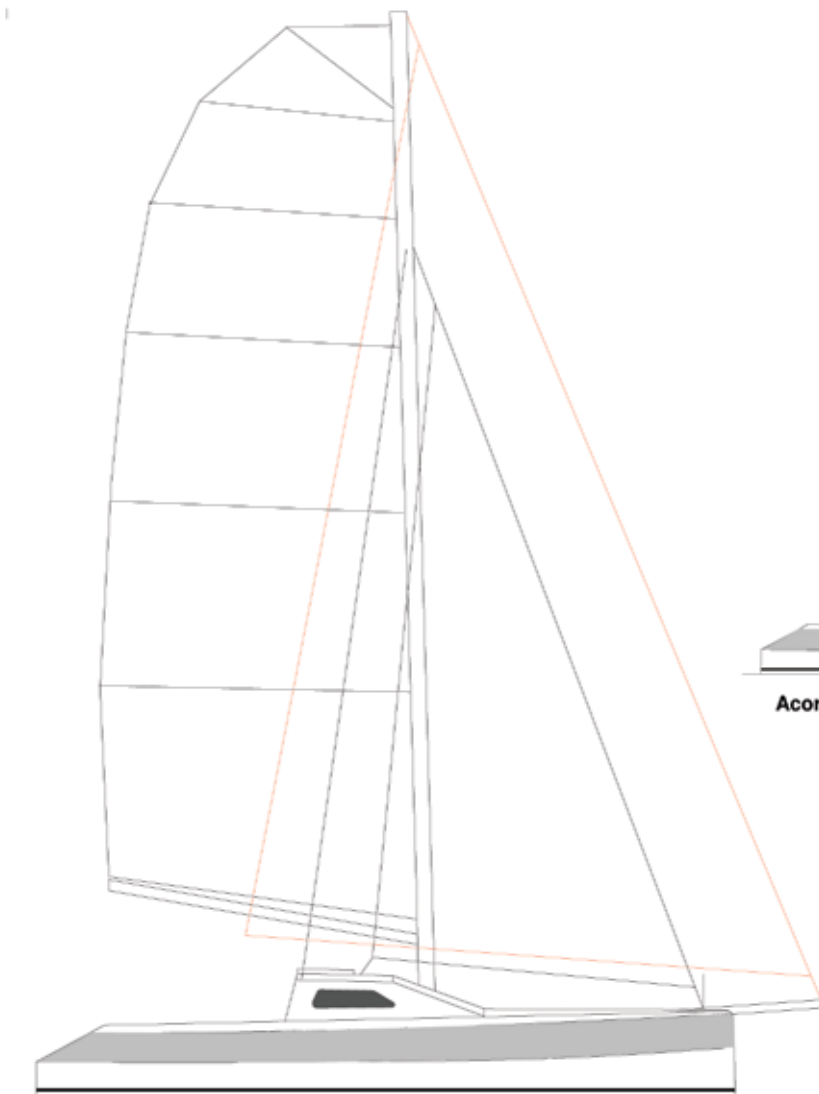
6mm ply	20 sheets
9mm ply	4 sheets
Timber	
2in x 1in 40m	
3in x 1in 2m	
2in x 2in 5m	
1.5in x 1in 40m	
Keels	1850 x 300 x 4off
Beams	4in x 2in x 2.5m 5 off
Rudders	3m x 8in x 1in 2 1.5in x 2m
Glass tape	150m 100mm (4in) wide
Sheathing (optional)	60sqm 200g/sqm (4oz)
Epoxy	20kgs
Paint, filler etc as required	

A detailed line drawing of a sailboat, specifically a ketch, shown from a side profile. The boat has a long, narrow hull and a flat deck. It features two masts: a taller mainmast and a shorter mizzenmast. The mainmast has five rectangular sails attached to it, while the mizzenmast has a single, larger sail. The boat is shown on a simple horizontal line representing the water.

MEET THE **ACORN21**

**21FT STITCH
AND GLUE**

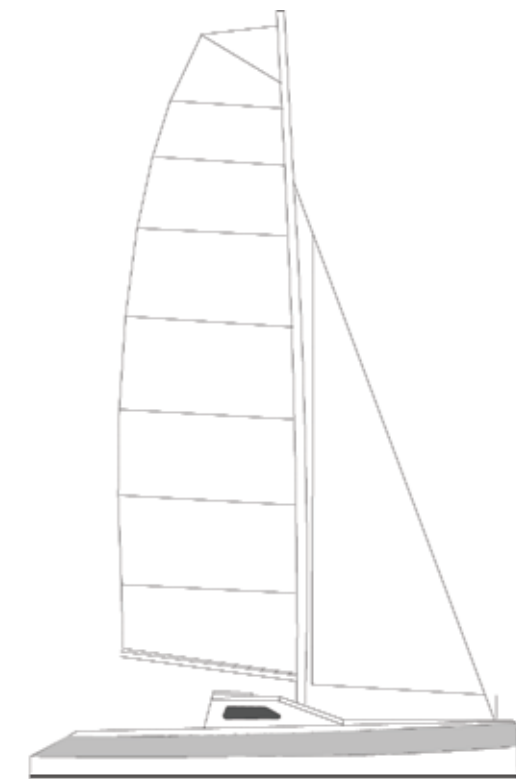
SEE THE SENSE
OF DESIGN,
SHOWN HERE



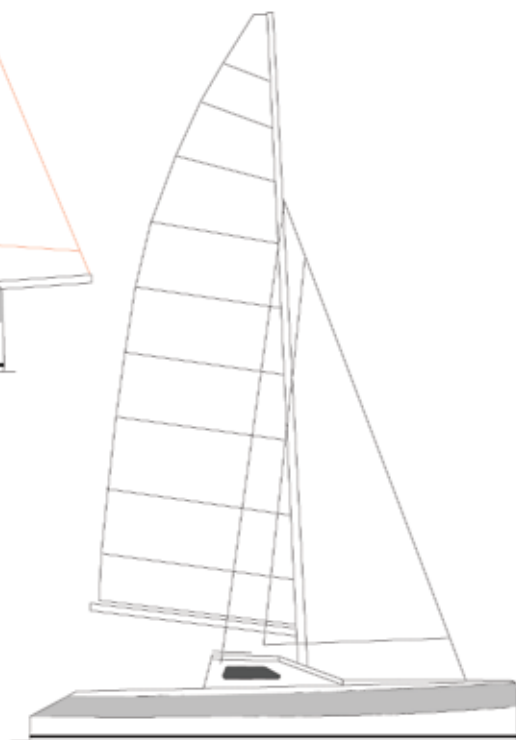
21ft Acorn
 simple, trailable, minimalist, performance cruising catamaran
 Large beach cat rig (plus fols, deck gear etc)
 can be used instead of (racing) rig shown here.
 Cruising rig also available

LOA 6.4m
 LWL 6.35m
 BOA 3.8m
 Racing Sail Area (as shown)
 Mainsail 19.6sqm
 Jib 9sqm (screecher in red optional)
 Empty weight 400kgs
 Displacement to WL 720kgs
 Draft 190, (1000 boards down)
 2 single berths in hulls plus boom tent
 headroom 1.3m

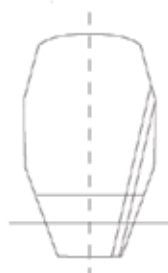
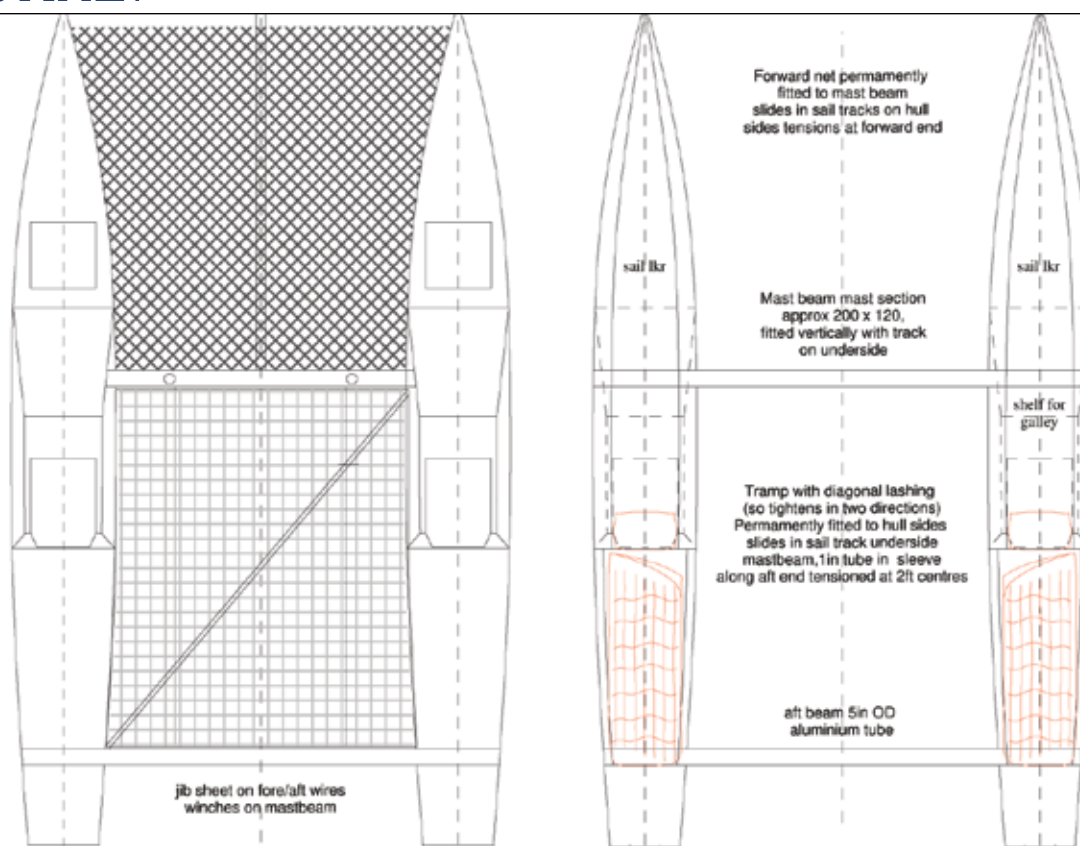
Acorn
 Study Plan
 Copyright WOODS DESIGNS
 August 2010



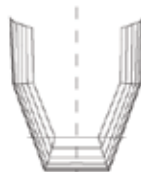
Acorn with modified Tornado rig (bigger jib fitted)



Acorn with modified Hobie 18 rig
 (mainsail luff is reduced to allow boom to be raised)



Hull Cross Section in Cabin

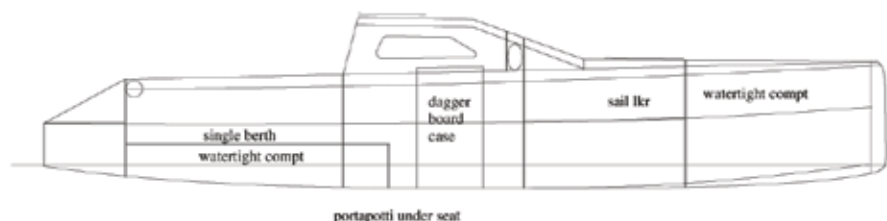


Hull Cross Sections (aft on left, fwd on right)

Note: For Clarity cross-sections are at a different scale from other drawings



Hull Cross Section Comparison with Janus (in red)



Hull materials:
 4mm ply hull sides approx 12 sheets
 6mm ply bulkheads, hull bottoms and decks approx 13 sheets
 Sheathing 200g glass and epoxy
 Chine joints are timber or glass tape/epoxy
 Approx 25kg epoxy min

BASIC MATERIALS LIST

PLYWOOD

Hull sides 11 sheets 4 mm ply (3/16in)
Hull bottom 2 sheets 6mm ply (1/4in)
Hull bulkheads 2 sheets 6mm ply
Hull decks 4 sheets 6mm ply
Daggerboxes, beams 1 sheet 6mm ply

TIMBER

100m (330ft) 2in x 1in
40m (150ft) 1in x 1in
20m (70ft) 1 1/2in x 1in
3in x 1in 5m (16ft)

EPOXY

25kgs epoxy
10kgs 200g glass
(Approx, excluding waste)

NOTES:

All plywood to be best quality marine grade Gaboon ply. Sheet sizes are 8' x 4', 2440 x 1220.

All timber to be at least 'joinery quality'. Unless noted otherwise all timber is softwood, eg Douglas Fir, Sitka Spruce, Yellow Cedar or similar.

All timber is 'PAR', or 'Planed All Round'.

Thus sizes given are nominal, ie 2" x 1" has a finished planed size of approx 45mm x 20mm.

(**Note:** It is usually cheaper to buy 2" x 1" and cut it in half to create 1" x 1".

Similarly cut 3in x 1in to make 1 1/2in x 1in).

Epoxy glue is recommended for all glue joints as it is the strongest and most watertight glue.

The Acorn is a 21ft lightweight plywood minimum fast cruiser that has been kicking around for nearly 20 years but only recently have plans been drawn.



An Australian boat a couple of weeks into the build.

Some parts, eg boards, rudders, spars, can come from large beach cats (ie Hobie 18 or larger). The rig shown is the optional racing rig which is only suitable for experienced sailors in lightwind areas. Smaller rigs are available, see the study plan

Now, we design all our craft to be easy to build, perform well, and not make much of a dent in the family budget.

I am convinced you can't build a better performing boat, and it is very cheap, light, and I am sure Mum and Dad and two small children will be quite comfortable overnighting.

You also have to remember the extremely light weight and also I would not like to waste the cockpit area for further accommodation.

Most of our cats are flat bottom with a wedged bow, for great, stable speed.

Surely someone out there can come up with an expandable trailer or extendable crossarms, (AKAs) and wow, another winning design.

Price of plans in Australia is \$349 downloaded, including our now famous VOUCHER for discount for life on all parts for this boat, when you choose to buy from 'All Boat Kits'.

I reckon without a special trailer or some other arrangement two hours will see you happily sailing away.

But you are all a bit smarter than us so once again contact us for a chat ... remember, this 21' Acorn is sooo cheap!

NOW, WE DESIGN ALL OUR CRAFT TO BE **EASY TO BUILD, PERFORM WELL**, AND NOT MAKE MUCH OF A DENT IN THE FAMILY BUDGET

I would rate this boat at four out of five due to delays at the ramp, or am I being too cautious?

Please call or email 'cos I am a lonely old bloke and love to talk boats.

This catamaran will really hit the spot and I am convinced is the boat we have all been waiting for ... very nearly a five out of five rating.

See you on the water, Barrie

07 3172 9642 or 0478 280 619

ba15@bigpond.com www.allboatkits.com

ARE YOU?

building a boat making oars refitting varnishing designing making a kayak updating electronics sailing
racing cruising selecting timber sanding fairing painting trailering buying selling sewing sails repairing
reviving marinising an engine building a kit customising launching building a rudder using epoxy making
a mast or simply dreaming about messing about in boats ...



If so, we'd like to hear from you!

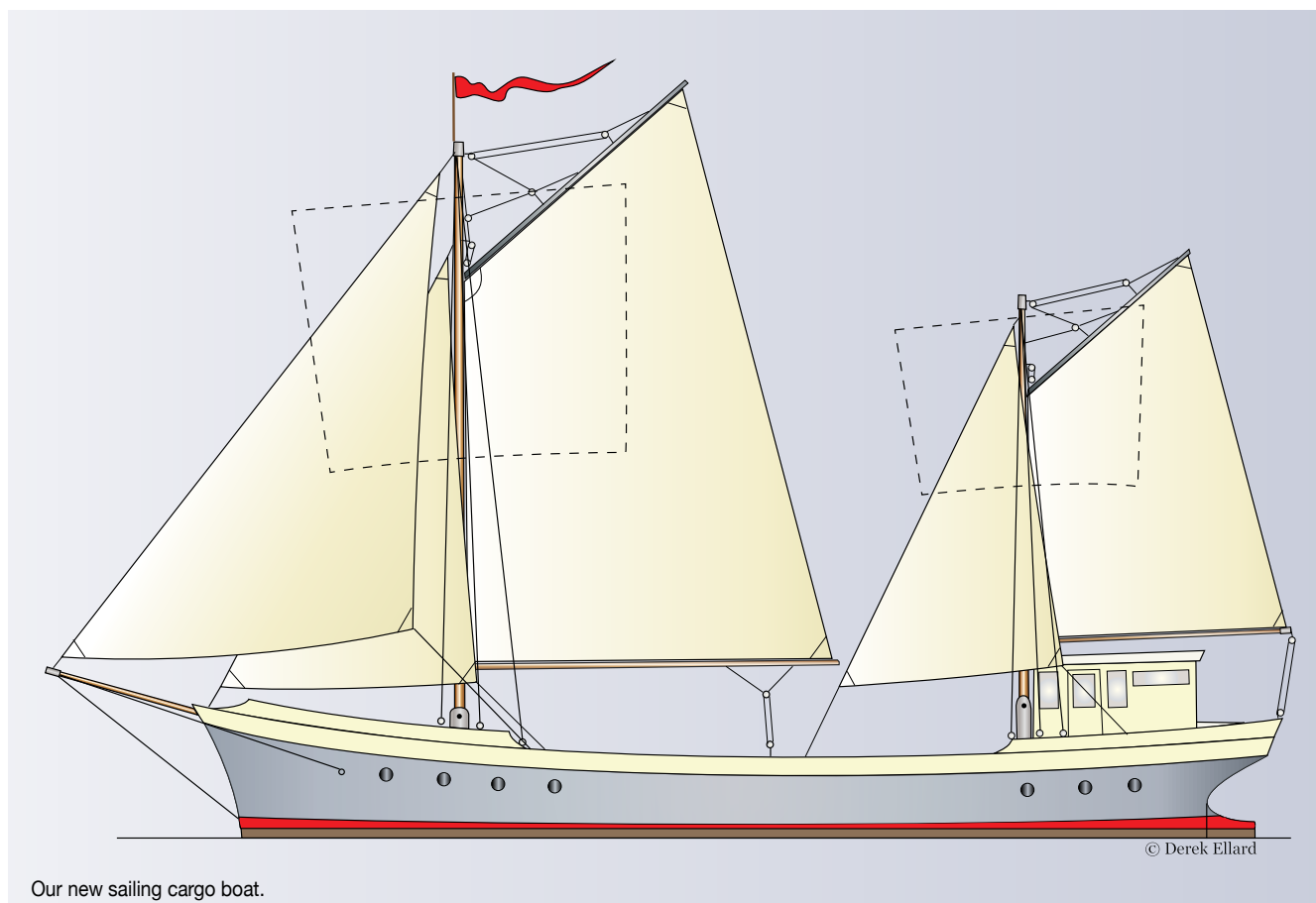
Australian Amateur Boatbuilder & KitBoats magazine is interested in hearing the story of your project, the reasons why, how you got started and what was your motivation. Don't worry if you can't write a masterpiece and naturally, you'll have to include some clear photos, showing the various stages of construction from the beginning to where it all ended!

PO Box 560, Varsity Lakes, Queensland 4227

07 5593 8187 or wendy@multihull.com.au



SCRUFFIEWORLDNEWS



Our new sailing cargo boat.

by **DEREK ELLARD** www.scruffie.com

GO SAIL CARGO

In the last issue I wrote about sailing cargo ships and their role in our brave new world. Well I've gone a step further and, prompted by Ben of Royal Queensland Yacht Squadron, designed a new two-container coastal cargo boat.

The transportation industry is undergoing a revolution with all the big players, without exception, investing heavily (and sometimes fruitfully) in electric power. Sadly, the Australian government with its unhealthy coal and oil obsession, is light years behind. As

the planet sleep walks into an environmental Armageddon, we're going to give a billion dollars to an Indian corporate criminal conglomerate to stash in the Cayman Islands.

No coal on my new boat, and there's Buckley's chance of any Cayman collateral either, but there is a powerful Ketch rig with twin topsails and a big bank of batteries powering twin electric motors. Lots of solar too, and cabins for paying passengers.

The boat is to be built in steel and available as a kit. Don't laugh, I'm serious! Of course a 90ft steel cargo

boat is not going to fit into your average suburban carport but modern technology means that a kit of precision steel panels is easy, so once we get the first one sorted we'll be able to work up a comprehensive digital building manual for small shipyard anywhere.

It's been a labour of love to work out all the details from carbon fibre Squaresail yards to cargo handling gear. The hull is very similar to thousands of other small cargo vessels but the sailing requirement meant that great care had to be taken in achieving an easily driven hull that was stable and capacious, handy to windward and able to safely take the ground.

Efficiency and reliability were the key words so there's a crew of only three and the aim is for zero fuel costs. The important issue here is that the thing has to be a viable proposition, it's utterly pointless to design a lovely romantic square rigger which needs a crew of twenty and can't handle containers. From 50-tonne steel ships to 100kg Shimmys, the same principles apply – the vessel must be fit for purpose.

Details of the new cargo boat are available – email to discuss **derek@scruffie.com**

SHIMMY FINESSING

We will have finished and delivered another Shimmy by the time you read this, they are now firmly established as our most successful model. We've taken more steps to refine the build process, this time more work in pre-priming and pre-fabrication of all the parts in the never ending quest for an efficient, fast building procedure.



I've been framed.



Rudder keel interface.

The photos show various stages, most self-explanatory, but you can see we've built up coats of epoxy primer on the frames and bulkheads, sanding them back to matt prior to assembly. I glassed and primed the keel before assembly too, I do this for some kits as it's much easier to sand flat on a bench or trestles than sand a vertical keel on an already built boat. It all helps. Made sure the rudder was assembled and faired before priming the hull this time – I sometimes forget – so the hull and rudder can be coated together.

SOCKS – AN UPDATE

A couple of years ago I briefly reported on advances in sock technology noting that 'all day' brands had been introduced indicating that the lucky wearer could indeed now be safely socked from dawn to dusk. Remarkable. On building the new Shimmy I found a new use for the handy hose – the photos show used but still serviceable (kindly donated in a bag of rags) socks used to cushion a shaped fairing block for coving and as pads for temporary legs – socks, where would you be without them?



Support hose.



Sockcessful sanding.

AN IKEA ADVENTURE

Yes, we went to Ikea to buy a new office chair for the office manager and a couple of units for storing boat drawings and for the TV to sit on. Now Ikea is one Swedish thing we all know about – kit furniture with quirky names like 'Slattern' and 'Groening'. Annette's new chair is a sturdy steel job called Roberget. Annette is happy sitting on Roberget whereas I am not. After several hours we managed to find our way out, others weren't so lucky I hear, ending up as stateless shoppers in a sinister Scandinavian time warp, forced to eat iced herring smorgasbord in a sterilised cupboard for eternity. I'm not going back there without a pocket GPS.

MODS TO MODERNISE

Had an enquiry about modifying an old Scruffie 16 a few weeks back and I thought I'd comment on modifications to our boats. They are different to almost all others and follow a different set of rules. Little ships rather than big dinghies with low aspect lugsail or gaff rigs and fixed keels.

Now the earlier 16's – and I'm going back 20-25 years – had shallower keels and simple rudders which meant they weren't too efficient to windward. The shallow draught hull was easy to beach and the single lugsail fast and fool-proof to rig but I've never been one to stand still so keels became deeper, rudders NACA foiled and fitted right up close to the keel to reduce turbulence to a minimum. More lead was poured in the keels, the lugs were peaked up, battened and roached. The bowsprits were standardised as was a furling genoa. Add a host of minor tweaks and by the late 90's the 16 had evolved as far as it could without unacceptable compromises.

Almost all the earlier models have now been upgraded and refurbished but there's still one or two original spec boats out there, I'm sure.

Of course you wouldn't expect a 20-year old Holden to perform like a new Toyota would you? So when I'm asked about fitting lee boards or centreboards, or lifting rudders I say don't do it. Two or three old Scruffies and Stornaways have had different rigs fitted too. The last one I knew of was a Stornaway, acquired by Jono of the Blue Peter Sailing School. He runs a couple of Siennas, three or four Stornaways and a few assorted centre boarders and the 'modified' Stornaway was 'left for dead' by the rest of the fleet.

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It's true to say that a well-designed centreboard dinghy with a Bermudan rig will out point even a Sienna but that's missing the point. The Sienna will comprehensively out sail the Bermudan rigged dinghy in heavier weather and do it in safety. The rig and hull are carefully designed to complement each other and the result is a boat ideally suited to its purpose – a boat to seriously voyage in – to boldly go, even.

If you don't want one of our designs then there are thousands of 12-20ft skiffs to choose from, most with centreboards, mostly with flattish sections and available with a variety of rigs. Many are skittish, many prone to capsizing but the better ones are perfectly suited to their intended sheltered waters, they are often cheap too. Our boats are not for everyone but if you want to mount an expedition or undertake some serious exploration, then a little ship with a ballasted keel and an efficient easily handled rig is for you. If speed is also necessary then our Secret 20 will out sail all sorts of boats and the Sienna 19 will, as Jono puts it, "sail through the fleet."

MORE ON RUDDERS



Barn Door – or should I say barge door.

The traditional keel hung rudder is clearly illustrated in the drawing of the Thames Barge J & M – it's one that will do the job year in, year out, but in terms of manoeuvrability and enhancing windward performance, well no. A couple of decades ago the wonderful J Class racer *Endeavour* was being restored. On sailing her she was found to be 'almost unmanageable.' The solution was to fair the rudder and add flaps to close the gap between the keel. The result? A boat transformed, apparently.

MORE WORDS ON CARGO BOATS

I've been re-reading some of my books on sailing barges, one in particular *East Coast Voyage* about a man, his wife and young son sailing a barge from the Humber to Deben rivers – a three day voyage undertaken in the 60's. I was reminded in no uncertain terms of the sheer audacity of it – sailing an engineless 80ft sailing barge down the east coast of England with your wife and son? Wow – respect! I know these waters and I'm in awe. Not only that but prior to the voyage they spent months fitting her out and then there's the small matter of shaping and rigging a new 42ft solid fir topmast before setting sail. An inspiring tale and a link to a rapidly disappearing past. There's probably a few copies in better libraries but I bought mine in 1971, so maybe not. Anyway, an *East Coast Passage – the Voyage of a Thames Sailing Barge* by D. H. Clarke, Longman Group, London.

Here's an extract to whet your appetite.

'Could I take the wheel?' Mollie asked.

'Not a hope. I can barely hold her myself.'

They went forward along the weather deck—the lee rail was mostly submerged in bubbling surf—and edged down the sloping mast-deck to the winch. The invading sea swirled round their feet. They fitted the winch handle—Mollie back-to-rigging, Kester back-to-mast—and tried to crank. They did not even crank one pawl.

'Hang on!' I shouted.

I waited for a chance, fiddled the wheel, waded into the scuppers, unhitched the mainsheet from the spiked block and released a few feet. Because there was so much wind, the sail remained as full as it was before.

'Now try,' I called.

This time they managed to crank a few pawls.

Then I released a little more mainsheet. . . .

Then they cranked a few more pawls. . . .

Every so often, Kester had to overhaul the middles and lowers to prevent them from snarling up, whilst Mollie leaned breathlessly against the winch. Kester did not have the weight to do more than this with the brails, but it was a help. As the main brails dragged the mainsail inch by inch towards the mast, and the pressure of the wind on the sail lessened, so J & M came more upright. Bit by tiny bit. . . .

It took forty long, long minutes. . . .

Lastly we at Scruffie Marine fit brailing lines to all our lugsails but ours are brailed in seconds ...

THOUGHTS ON **TURNING** **BOATS**



by **TONY O'CONNOR** www.oconnorwoodenboats.com

The majority of boat designs for amateur builders nowadays are built upside down. There are good reasons for this, gravity is your friend, and long panels and planks can be laid on the building frame for fitting, especially for shorthanded work as there is no need for complicated sticks to shore up planks. Also sanding fairing and painting an upturned hull is far easier.



Easing the boat down.



Jewell being turned in custom frame.



Finished the job.

One of the things you will have considered is do I have enough room to build a boat? In some ways that's only half of the question – because the other half is do I have enough room to turn a boat?

If your boat is small this can be an easy process. A small dinghy can be carried out onto the lawn and just rolled over. My adult son and I easily turned a Kernic boat built for the naval cadets in Western Australia. Kernic was about 100kg and 6m long – so relatively light when we turned it.

We didn't need any special gear, but we did need to do some preparation so the boat didn't get damaged in the turning process or in its new position. We screwed a sacrificial piece of pine to the sheer plank where the hull was going to be resting against the floor. This would ultimately be replaced by a rubbing strake in a lot of cases the sheer strake isn't painted at this stage but all the other planks have been, so if it does get damaged, this can easily be repaired later and the sheer plank is easily

accessible for painting. We covered the floor with old cushions and doonas where the boat would be in contact with the floor as it rolls over. Because the boat was light, one of us stood at each end and tilted the boat up until it was on edge, until we felt it was at the point of balance, and then then carefully walked around to the underside and slowly allowed the hull to roll down to its final position.

With a heavier boat, you need to plan things out a little more. Rory and I also turned Jewell which was

AS THERE WERE ONLY TWO OF US I BUILT A TEMPORARY FRAME TO STRADDLE THE HULL TO ALLOW JEWELL TO ROLL IN A MORE CONTROLLED MANNER

about 500kg and again 6m long. But by the time it was turned the internal ballast was in the keel, and this gave the hull a very strong righting moment – that is – the she wanted to go over when she reached a certain point. As a result, we used lifting tackle off the roof beam and a horizontal retaining rope passed around the leg of a work bench attached to the wall to control the turn. In that situation due to the strong righting moment the rope ended up taking more strain than I'd anticipated, and began to pull the bench away from the wall. As there were only two of us I built a

temporary frame to straddle the hull to allow Jewell to roll in a more controlled manner.

This month, we turned the hull of the 10m Francois Vivier designed Pilbara schooner. Although the hull weighed about 400kg, we couldn't use the lifting gear because of her length and how the workshop has been organised for this build. So I put the call out to the my comrades in the Old Gaffers Association in WA, as well as family and friends and had 12 able bodied people on the day. I'd prepared by giving the workshop a really good tidy up – moving all machinery that could be moved out of the way as far

as possible. I also gave the floor a good sweep because we'd need our shoes to have good purchase on the floor. Unfortunately, the protective steel belting for the bilge runners and skeg and forefoot weren't ready in time, so I screwed a couple of sacrificial pieces of pine to the bilge runners and taped old carpet to the skeg and forefoot by way of protection as well as a pine

I ALSO GAVE
THE FLOOR A
GOOD SWEEP
BECAUSE WE'D
NEED OUR
SHOES TO
HAVE GOOD
PURCHASE ON
THE FLOOR

Hull turned and
ready for work.



wearing board along the sheer plank where it would rest on the floor.

The day before, I used a trolley jack to raise the hull off the building frame and rested it on some crates, then from underneath I disassembled the entire building frame and removed it.

Then I attached two retaining ropes to the internal structure. On the day of the move we lowered the ends back down onto the floor, and began to raise the boat up on its side. Just before we reached the balance point, we realised we were going to have to walk her sideways across the floor as the workshop wasn't wide enough

for her to just be rolled. This was done by raising one end and moving it across the floor, then lowering that end and raising the other end and moving it across, and so we slowly shuffled the hull back across until there was room to lower her. Video can be seen here; <http://www.oconnorwoodenboats.com/videos/>

Things were kept stable by keeping the hull just before the balance point so she always wanted to go back down to her original position and having most of the team on that side of the boat until we had enough room to roll over.

When we had enough room, it was time to finish the turn. We raised the boat to its balance point. Unlike, Jewell, once this boat reached the point of balance she was reasonably stable, so there was time for most of the team to move to the other side of the boat

THINGS WERE **KEPT STABLE** BY KEEPING THE HULL JUST BEFORE THE **BALANCE POINT** SO SHE ALWAYS WANTED TO GO BACK DOWN TO HER **ORIGINAL POSITION**

to be there to ease her down with a couple of guys couple holding the retaining ropes. And then carefully we rolled her down onto some padding.

When turning boats the main advice I would give is to have a plan beforehand, taking into account the size and weight of your boat, the room available, and whether there is internal ballast.

As a rule the more hands the better, as long as the people assisting are aware of what is needing to be done, the last thing you need are people getting in the way .

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For more information about Jewell or any other of Francois Vivier CNC kits contact Tony O'Connor at



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www.oconnorwoodenboats.com

AUSTRALIA'S WAR AT SEA, 1914-1919: **THE NAVY**



by **DAVID JONES, QUEENSLAND MARITIME MUSEUM**

Queensland Maritime Museum is currently commemorating Australia's nautical involvement in the First World War with the Australian National Maritime Museum's travelling 'War at Sea' exhibition. Presenting first hand stories, artefacts and personal memorabilia, the exhibition brings to life the personal experience of Australians during the war at sea a hundred years ago.

In the early years of the 20th Century Germany strove to expand its influence on the world stage, which included an intensive naval building programme to challenge Great Britain's supremacy at sea. In the arms race that followed the infant nation of Australia saw to it that we possessed a modern navy capable of defending our shores against enemy attack.

Thus when war broke out on August 4, 1914 the Royal Australian Navy was ready with a powerful new fleet unit based in Sydney. Pride of the fleet was the battle-cruiser *HMAS Australia*, up-to-date, fast and armed with the latest 12 inch guns. She was supported by modern cruisers, destroyers and submarines.

German interests in the Pacific were widespread, from northern New Guinea and New Britain to a chain of scattered island groups further afield. To guard them Germany based a powerful squadron of armoured and light cruisers in the Far East which cruised their island possessions.

First task of the Australian Navy on the outbreak of war was to eliminate the German squadron and Australia immediately led a raid on Rabaul harbour hoping to surprise any enemy warships in port. But the bay was empty.

Far away from home and friends, the German Admiral Graf von Spee took a cautious approach. He considered the guns of Australia more powerful than

LEFT: HMAS Australia in Sydney upon the first arrival of the Australian fleet on October 4, 1913. Image RAN Heritage Centre collection

RIGHT: Transport Sardinia in 'dazzle' camouflage intended to confuse U-boat attackers. Image Australian National Maritime Museum collection



his two armoured cruisers combined and he kept his squadron well away from her, eventually fleeing the Pacific by the end of 1914.

Nevertheless, Spee's very presence in the Pacific seriously concerned allied leaders who demanded a strong escort for any troop movements at sea. The Australian fleet was redirected to protect troop convoys in capturing Samoa and occupying German New Guinea, both tasks being rapidly accomplished with the Navy's help.

Most important was guarding the despatch of 30,000 Australian and New Zealand troops and their horses to the centre of conflict in Europe. A heavy escort was provided for the ANZAC convoy, and this paid an unexpected dividend. As they passed Cocos Island on November 9, 1914 a wireless message reported a strange cruiser had appeared at the island and a landing party was being sent ashore.

HMAS Sydney was detached from the escort to find this was the German cruiser *Emden* which had

caused considerable damage to shipping in the Indian Ocean. The battle that followed in which Sydney destroyed the *Emden* is celebrated as the Royal Australian Navy's first victory at sea.

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By the end of 1914 all the German warships overseas had been accounted for and Australia's fleet could be released for areas of greater need. *Australia* went to British waters, *Melbourne* and *Sydney* following her there after a period patrolling off the West Indies and North America.

Heavy guns had been the foundation of naval power for hundreds of years and the arms race preceding the war concentrated on building more and bigger battleships with heavier and more powerful guns. By 1914 Great Britain and Germany possessed enormous fleets of dreadnought battleships and battle-cruisers which faced each other across the North Sea.

HMAS Australia joined the vast concourse of the Royal Navy's Grand Fleet in February 1915. For the next four years her war became a long and tedious series of patrols and exercises in bleak northern waters, always prepared and ready for action.

But for *Australia's* flagship that battle never came. It was her fate to be under repair in dockyard hands on the one occasion in all those years when the two opposing fleets finally met at the Battle of Jutland in June 1916. Results of the battle have long been

debated, but out of the British battle-cruiser force to which *Australia* belonged, three of her nine sisters engaged were sunk with the loss of 3,300 lives.

HMAS Australia's moment finally came 10 days after the Armistice when a defeated Germany surrendered its battle fleet for internment, sailing to Great Britain between two long columns of the Grand Fleet's dreadnoughts. *Australia* was given the honour of leading the port column of the battle fleet in recognition of this nation's outstanding contribution to all aspects of the war, not least in the war at sea.

The enemy battleships remained a dangerous threat for the rest of the war, but largely inactive. Instead, Germany gave increasing priority to submarine warfare. Submarines were an untested weapon prior to the Great War but they quickly showed their ability to sink large, armoured warships and disrupt merchant shipping traffic. German U-boat numbers rapidly increased and the introduction of an unrestricted warfare policy early in 1917 was so successful that Great Britain was almost isolated from outside resources.

Destroyers were badly needed to defend convoys and combat the U-boat menace, and Australia's six destroyers were transferred

to the Mediterranean particularly for anti-submarine duties. They worked hard, engaging enemy U-boats on several occasions with depth charges and gunfire, and aiding damaged ships. To assist in this role they were equipped with underwater listening devices and an observation balloon to widen their vision and add the possibility of sighting submerged U-boats.

Australia entered the war with two submarines of our own. *HMAS AE1* and *HMAS AE2* were the latest of their type and in their delivery passage during 1914 completed the longest overseas voyage by submarine up to that time. Sadly, both were destined for very short lives. *AE1* tragically disappeared off Rabaul on September 14, 1914 with the loss of all hands during the Australian capture of German New Guinea.



TOP: Australian submarine *HMAS AE2* with crew on deck before leaving the UK for Australia, 1914.
Image Australian National Maritime Museum collection



ABOVE: Destroyer *HMAS Warrego* in Brisbane while patrolling the Australian coast in 1915.
Image RAN Heritage Centre collection

AE2 was sent to the Mediterranean and on the day the ANZAC's landed on the beaches of Gallipoli she was creeping through the Dardanelles to attack Turkish shipping. Previous attempts to penetrate the strait had failed, but *AE2* was the first submarine to succeed, and news of her achievement signalled that night was a great encouragement to the embattled troops ashore. Unfortunately, her success was short lived as four days later *AE2* was sunk and her crew taken prisoner.

Aviation was another new and untested science at the beginning of the war, but technology and experience improved rapidly. While surface action eluded the great battle fleets in the North Sea both sides sought ways of bringing aircraft into the war at sea.

Germany possessed a fleet of Zeppelin airships which were used for scouting purposes and Sydney had an inconclusive battle with one of these in May 1917. Later that year *Australia*, *Sydney* and *Melbourne* were each equipped with an aeroplane launching platform. They showed their potential when *Sydney's* aircraft forced down a German reconnaissance aircraft in a short pursuit over the North Sea.

At home the Navy, through its Naval Brigade of reservist personnel and cadets, was heavily engaged in port security and just a few vessels remained to undertake patrol work. The older cruiser *HMAS Encounter* was the most important of these, and she was assisted by a few smaller and auxiliary warships. Troopships were despatched from Australian ports throughout the war and only once, during 1917, did a German warship enter Australian waters. But each of these is a story of its own.

Concurrently with the formation of the Royal Australian Navy the Federal Government set about establishing the capacity to build warships locally. Before the war the destroyer *HMAS Warrego* was assembled from prefabricated parts at Cockatoo Island Dockyard in Sydney. Three more destroyers and the light cruiser *HMAS Brisbane* were built from the keel up at Cockatoo Island and these four Australia-made warships joined the fleet in 1916.



TOP: *HMAS Sydney* in the North Sea with an aircraft platform over her bow gun, 1918.
Image RAN Heritage Centre collection

ABOVE: All that was left of the *Emden* after her battle with *HMAS SYDNEY* on November 9, 1914.
Image Australian National Maritime Museum collection

The history of the First World War tells of great and costly campaigns climaxing after four years in ultimate victory. Australia's young Navy served faithfully throughout the conflict making a distinguished contribution to its final outcome. But it was the men who made the Navy, and their personal stories presented in the 'War at Sea' exhibition give life and colour to that history.



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LETTERS TO THE EDITOR



Dear Ed

Some good news for would be Jarcats builders: Alan Turner has completely redrawn Ross' plans to A3 format and has put them back on the market after several years absence.

He has also reworked the instruction books somewhat. Prices and plan availability can be found at jarcatyachts.com

These plans are only a sideline for him in an otherwise very busy business so I have undertaken to answer technical queries etc. for him.

I can be contacted at maintann@gmail.com

It may take a day or so for a response from either of us as the Jarcats plans are a secondary interest.

I'd suggest that potential builders look at joining the yahoo groupgroups.com/group/jarcats/

Many informative posts there about building, sailing and socialising on Jarcats. (There are also Facebook pages dedicated to these beautiful little trailerable yachts)

See you on the water

Don Nicholson

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Lemon Peel is one of the safer formulations for paint stripping. It employs less dangerous chemicals for stripping and removing paint, graffiti and for general industrial cleanup. Unlike older methylene chloride based paint strippers, Lemon Peel has much lower vapour pressure, that is, it has much less fumes evaporating from it and what does evaporate is not as dangerous as the fumes from the older style paint strippers.

Lemon Peel is effective against most coatings including acrylics, alkyd and oil based paints, single pack epoxies and of course anti fouling paints on boats. It will cause two pack epoxies to soften and given time will greatly assist in their removal.

Comparison tests of Lemon Peel have also been carried out against, US produced, 'Soy Gel', (one of the best, 'water based, safer to use', paint strippers that will work through multiple layers of paint). Our 'Aussie' manufactured, Lemon Peel stacked up very well and proved to be equally as good as, if not better than, Soy Gel.

In use, Lemon Peel is spread over the surface in a rather thick layer of 0.5-1mm. It can then be removed by scraper and finally washed down with hot water, detergent, a scouring pad and then hosed off with fresh water, prior to sanding and repainting.

The secret to enabling multiple layers of paint to be removed is keeping Lemon Peel moist. This is easily done by applying a generous layer (0.5-1mm thick) of Lemon Peel onto the area to be stripped and covering it with a sheet of plastic wrap to prevent drying. Under layers of Lemon Peel will stay moist for up to 24 hours and enhance penetration into the paint layers.

No toxic dust! No toxic waste! Residue scrapings can easily be collected in a dust pan, (or the like), placed in an appropriate bag or container for environmental safe disposal, according to local regulations.

Average of 1-2sqm per litre corresponding to a 0.5 to 1mm thickness.



Always wear gloves and eye protection. Latex gloves are suitable to resist 'splashes'. For more sustained exposure Butyl Rubber or Ansell Barrier are more suitable and should be worn.

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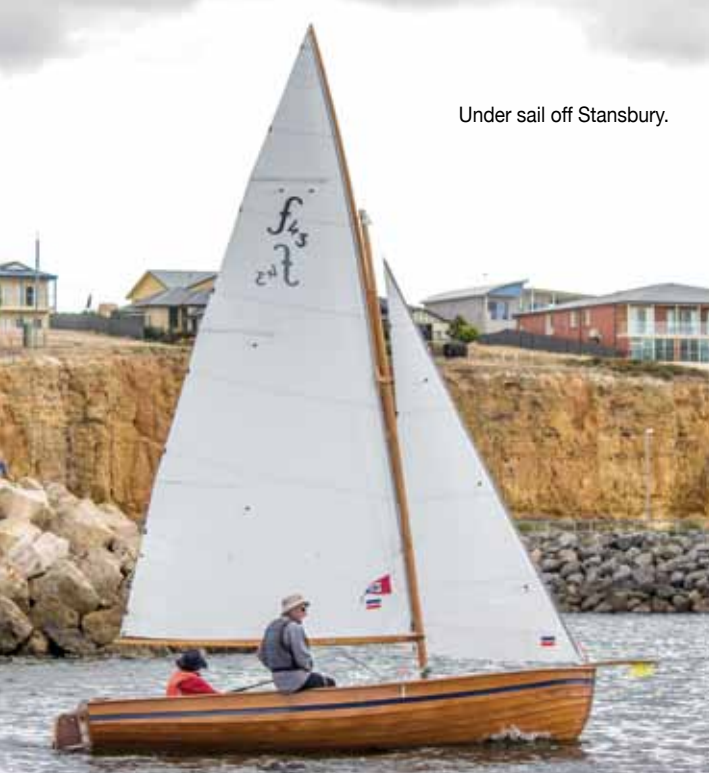


THE BOWS OF CLASSIC CRAFT
ARE TURNING TOWARDS

YORKE PENINSULA'S SALTWATER CLASSIC



Wooden and classic boat owners are varnishing timbers, polishing brass and servicing their old engines in preparation for Yorke Peninsula's Saltwater Classic on the weekend of April 21 and 22, 2018.



Under sail off Stansbury.

The sister towns of Stansbury and Port Vincent will again host the Saltwater Classic event for enthusiasts and admirers of traditional craft that have been faithfully restored to keep afloat some of the great traditions of boat building.

The biennial event, which started in 2003, attracts boat owners and crews from across Australia.

With support from the South Australian Tourism Commission, Yorke Peninsula Tourism, local business and community groups, the biennial Saltwater Classic has also become an important tourism event for the region that is a coastal playground less than three hours drive from Adelaide.

In 2016 it attracted more than 60 wooden and classic boats with owners and crews along with an estimated 4,500 visitors to the towns and wider region.

The two-day Saltwater Classic centres on Stansbury on the first day and Port Vincent on the second. Apart from grand parades of boats in the bays off both towns, there is a coordinated run of vessels from Stansbury to Port Vincent, a highlight for boat owners

PROGRAM OF EVENTS

STANSBURY

Friday, April 20

6.00pm Welcome reception for registered entrants at Stansbury Sports and Community Club

Saturday, April 21

9.00am Foreshore display of boats, vintage cars, motor bikes and stationary engines and opening of Stansbury Foreshore Markets

10.00am Model boats display

12.30pm Launch of boats at Stansbury

1.45pm Grand parade of boats in Oyster Bay, Stansbury

2.00pm Start of regatta run to Port Vincent. Boats overnight in marina.

PORT VINCENT

6.30pm Official dinner at Port Vincent Sailing Club

Sunday, April 22

8.30am Marina open to public for viewing of boats

9.00am Opening of foreshore markets, vintage cars, motorbikes and stationary engines, local art, craft and regional produce

9.30am Boats depart marina

10.15am Blessing of the fleet at the Port Vincent wharf

10.45am Official run of boats to the south east buoy. Model boats display

1.00pm Grand parade of boats in front of town with craft edging into the shallows for public viewing. Juniors aboard sail training vessels.

2.00pm Presentation of prizes

2.30pm Farewell function for participants at Port Vincent Sailing Club

For entry forms or more information, go to www.ypsaltwaterclassic.org.au



Gathering of boats for a grand parade at Port Vincent.



An event for enthusiasts of all ages.

and crews who enjoy the open water aspects of the event, and for spectators along the coastline.

The Edithburgh flotilla of the South Australian Sea Rescue Squadron will again coordinate the on-water activities along with support from members of the Australian Volunteer Coast Guard based at Port Vincent.

A spectacular display of foreshore activities for families will include the award-winning Stansbury Seaside Markets and foreshore markets in Port Vincent along with special presentations of regional foods and wines, arts and craft, vintage cars and motor bikes, classic caravans and stationary engines from the past.

The Saltwater Classic continues to value add to existing regional assets including food and wine experiences, aquatic recreation and water sports and nature-based activities such as visits to Innes National Park and Walk The Yorke.

The success of Yorke Peninsula's Saltwater Classic was celebrated in 2016 with medals in the Yorke Peninsula Tourism Awards and the South Australian Tourism Awards. It was also a semi-finalist in the 2017 Community Achievement Awards.



Yorke Peninsula's Saltwater Classic

*Stansbury and Port Vincent
21 & 22 April 2018*

Yorke Peninsula's Saltwater Classic is a special event for enthusiasts and admirers of wooden and classic boats.

The regatta is open to all sail and power boats, but with a focus on classic craft. The Australian Maritime Museum defines a classic boat as "any vessel with unique and enduring qualities." Put simply, beauty is in the eye of the beholder!

The biennial event attracts people from across Australia to parade their boats in the waters off the sister towns of Stansbury and Port Vincent, and it draws spectators to a variety of water-based and shore-based attractions. Yorke Peninsula's Saltwater Classic is a community-based event that has become a significant tourism feature on Yorke Peninsula. Isn't it about time you visited?

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NORTHERN IRELAND AND SOUTH AUSTRALIA



by **TREVOR GILL**

Jim Taylor is an Irishman now living far distant from his original home shores. But through his passion and craftsmanship, he has fulfilled a lifelong dream that has created a unique link between the waters off County Down in Northern Ireland and Yorke Peninsula in South Australia.

Born into a traditional fishing family in Killough, Jim migrated to Australia in 1960, but he regularly returns to the village that lies on the west side of Killough Bay rising and falling with the tides from the Irish Sea.

A carpenter by trade, he shared a close and enduring friendship with Harry Henvey, the late fisherman, master boat builder and caretaker of St John's Point Lighthouse in Killough.

Sadly, Harry passed away in October this year taking with him immense knowledge of boats and the sea. But before Harry was gathered from under the beam of the lighthouse that had swept over his life for 87 years, Jim had the joy of helping him complete the last wooden boat to emerge from his old, weather beaten workshop.

Almost 20 years ago, Harry and his friend Harry Magee constructed a perfect replica of a Killough yawl called the *Marian*, which was built in 1924 for local fisherman Robert Taylor, Jim's grandfather.

The original *Marian* was in the Taylor family until the 1970's when it was sold for further seafaring duty off Minerstown, west of Killough, before being donated to the Down County Museum.

Harry was commissioned through the Killough Community Association to build a replica of the boat, also named *Marian*, as a working showpiece of the past when a fleet of the yawls worked the coastal waters off County Down.

The Killough yawl, based on a traditional Norwegian design dating back to the days of the Vikings, entered Irish coastal culture about two and a half centuries ago. The shallow draft yawls were 20ft long and capable of carrying six fishermen working oars or sails before the advent of outboard motors.

The boats were worked up and down gravelly shores upon greased wooden slats and they would venture far out to sea for mackerel, herring, cod and haddock. If the wind did not suit, the fisherman took to the oars, sometimes rowing for 10 or 15 nautical miles.

The last of the yawls disappeared from around Killough about 25 years ago, and their floating heritage could have been lost forever without the skilful intervention of Harry Henvey and Harry Magee.

Now, Jim Taylor has pitched in with his own dedication to the Killough yawl builders and fishermen by constructing his own replica of the *Marian* in a shed in his adopted town of

Stansbury on the eastern shore of Yorke Peninsula in South Australia.

While there will be shakedown cruises in the waters off Stansbury, the South Australian edition of the *Marian* will be officially christened in time for Yorke Peninsula's Saltwater Classic in April, a biennial event for enthusiasts of wooden and classic boats from around Australia.

"I have built her for my grandchildren as a labour of love that preserves our family links with Killough, my original home village," Jim explained.

"She is true to the design of the Killough yawls with her sweeping bow and slender beam, and I made her in the same manner as the boat that I helped my old friend Harry Henvey to build when I was there last year.

"My *Marian* is constructed of European larch and jarrah and instead of being a traditional double ender I have made a square stern to mount an outboard motor. But she will be fitted with a lugsail and it will blossom over the boat whenever the weather is kind.

"Sadly, I was travelling around Australia when Harry died and I did not know of his passing until after his funeral in Killough, which was attended by around 300 mourners.

"Harry lived his whole life under the beam of St John's Point lighthouse. He was a humble, but highly skilled man with calloused hands from working with wood to make real boats the way they have been constructed for centuries.

"While the *Marian* made in Stansbury is my tribute to Harry, to my forebears and to the village of Killough, my grandchildren will be able to use her to experience the way their ancestors went to sea. She is a safe boat, and a good boat that will stand the test of time."

So, Jim Taylor's labour of love truly gives strength to the famous old words: *Though the seas divide us, hands across the sea.*



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ONE SCHOONER, THREE SKIFFS AND TWO NORWALK ISLANDS SHARPIES

by **ROBERT AYLIFFE**

The old thief, time, runs faster when deadlines approach. Like I'm running now. Fortunately, the speed with which it's possible to roller paint even very big boats with Aquacote paint helps!

The RIG (Rebuild the 'Independence' Group) at American River on Kangaroo Island (SA) already have the first of their four St Ayles Skiffs painted, ready for turnover. The actual painting time was about four hours, from wash down to bright, shiny hard finish.

We had a good start on a clear blue day, the idea was that since the Bote-Cote mob's brilliant Aquacote

paint had never been used before on Kangaroo Island, it might be a good idea to workshop it with the program's team leaders, Grant Millard and David Hinves.

We got off to a good start by agreeing that the hull was well prepared for painting.

The sanded Bote-Cote is the perfect undercoat for Aquacote.

It was blowing pretty strongly, and rain was a slight possibility. There was quite a bit of dust in the air when the shed doors were open. We shut them only a bit.

Our equipment was adequate, the same short nap rollers that are standard for applying Bote-Cote, a couple of rags, some reasonably clean baked bean tins, a couple of small tongue depressor stirring sticks, some good masking tape and some old rags.

We began by washing the hull to clear away any major dust particles and to ensure that it had no previously unnoticed epoxy runs or dings or coarse sander scratches anywhere.

The Bote-Bote sealed hull, sanded and washed down ready for Aquacote painting.



The RIG Shed at the Wharf in American River. You can see the skeleton of the community built 'Independence' as she grows in the shed. You can just see the newly painted St Ayles Skiff Penneshaw in the foreground.



We stirred the paint in its one litre can. We added about a half a cup of water. The paint as it comes is fairly viscous and we felt that with the wind as it was it might other wise become too viscous, too quickly while working it.

Once it was good and runny, we poured into two roughly 400ml baked bean cans. We stirred in to them about 40 drips of 'cross linker', a critical step in the system for achieving Aquacote's legendary hard yet flexible finish.

When rolling with Aquacote, by the time you have painted from the boat's pointy end to the blunt end, you will probably be okay to start with the next coat. In warmer weather you can even sand within an hour of coating, if that's needed. This time of course it was the pointy end to the pointy end, and we did not need to sand between coats.

WHEN ROLLING WITH AQUACOTE, BY THE TIME YOU HAVE PAINTED FROM THE BOAT'S POINTY END TO THE BLUNT END, YOU WILL PROBABLY BE **OKAY TO START WITH THE NEXT COAT**

Grant started rolling on one side, and David, using a cheap brush pre-painted anywhere that was inconvenient for the roller, such as the tight curves of the plank land EGlue fillets, and the similar filleted juncture between the keel (deadwood) and the planking.

We did not use paint trays. Since Aquacote in wet film form cures very quickly, splashes become film become loose on the side of the tray, and become VERY annoying, falling back into the mix, and then onto the unsuspecting roller and then onto the job!

A good job, reliably and frequently spoiled by using paint trays.

What to do, what to do?

Easy! Another StrayDog discovery.

We save up offcuts of ply around 300 x 200 or something like that to make Aquacote palettes. We pour from the can as required and roll our paint before it flows off the edge of the palette.

Mostly.

I think it was David that got a bit slack and forgot it was just a flat palette, without sides. He managed to take home a nice white Aquacote sock, see pic. below.



Note David's smile of 'OH NO!' revelation! Palette MUST be held more or less level, or else you will get paint, as David Hinves found out, in your sock! First coat, note shadows. Subsequent thin coats fix that quickly.

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It works perfectly (accept for painted sock syndrome), and you can keep reusing your palette, safely painting over the layers of dried Aquacote. Gets a bit heavy after a while, is all.

It only took minutes before the 'this stuff is good' penny dropped. By the time we finished questions were being asked about use over existing boats including older wooden boats and glass and alloy ones too.

The answer is of course, yes. Contact StrayDog, or your local Bote-Cote agent for more details.

Combined with the water based epoxy Aquacote epoxy primer, it's super good on Alloy. Not surprising, since the technology was first devised for aircraft use, in the US.

We finished the off white over all the planks except the sheer, which is British Racing Green in actual. (We had some excellent distractions) work time of about 2.5 hours.

We had a fair bit of discussion between coats. In including, 'how many coats'?

Answer? Enough!

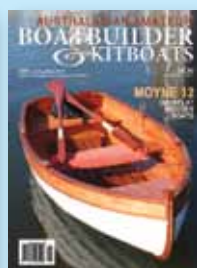
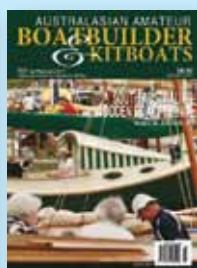
When any substrate shadows have vanished is my usual answer. Once that is achieved, if you want extra shine, then a light wiping over with Scotch Brite to remove nibs, a fresh water wash clean and then wiping over with Aquacote watered down by about 25%, with 50% extra cross linker, using a



The first Kangaroo Island St Ayles Skiff with her Penneshaw Aquacote livery. Aquacote brilliance; Off White hull, British Racing Green on the sheerstrake. Not bad for a first go, congratulations to Grant Millard and David Hinves!

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rag. This should be done quickly, carefully and with completeness, but just wiping. DONT try to use a polishing action.

This should make it really shiny, and really smooth. It will dry very quickly in this form.

Once finished, the next question was, do we chuck what's left in the cans into the bin, like we do with conventional two pack paints, because it just sets off and we can't use the balance?

Here is another Aquacote trick. After four hours, more or less, the cross linker loses effectiveness. So we waste nothing. That excess paint may be poured straight back into the can. (The corollary is that the cross linker must be topped up and mixed in after a few hours, as you proceed with your painting, to maintain good hardness later)

Recoating without sanding is possible up to four hours after curing. After that it should be lightly sanded first.

Finally, it actually seems to get shinier as the years go by.

IN THE SAME BOAT' SKIFF BUILD

Skiff Build, Clayton Church, Adelaide

Thought the 'In The Same Boat' Spire Community Group in Norwood (SA) would have their first ST Ayles Skiff planked by now. The first planks actually went on this last week, a big milestone.

The amazing 'Geelong Veritas' and 'Lifeboat' groups are leaving everyone else in Australia right now in a cloud of sawdust and planking. They are fitting the seats ... whoops, that one bucks the trend! to their boats in under a year. Look at Geelong Wooden Boat Festival, there will be St Ayles event, for sure.

Much more next issue.



Ruth and Max screwing on the first plank.



Gwenda and Max prepare for subsequent planks.



Max and Ruth finesse the completion of fixing the garboard planks. It's all easier from here!

IN THE SPIRIT OF CHARLIE FISHER

Turning an old NIS Mk1 into a hybrid Mk3.

Thought I'd have got my 'new' boat closed up before Christmas. That's not happening. Maybe Easter?

I kind of inherited the boat we built in my earlier days for Angus Houston, and dragged her then rather sorry collection of parts back from Lake Macquarie to my shed in Mt Barker.

Then, I got really busy, on everyone else's jobs.

My wife Ali decided I was to have an iPhone amputation every Thursday from 8am to dusk, while I did nothing but work on the as yet un-renamed boat. BRUTALLY painful as that was, and with the consolation help of long time friend, and employee in a past life, the incredibly capable Morgan Clark,



On the road late 2014.



Work in progress.



'Lemon Peel' at work, after 30 minutes.



After initial sanding.

we have made enormous progress over this last six months.

Carbon masts, tabernacles and booms. Slightly lengthened and raised cabin top. Revised interior and cockpit arrangements for long distance voyaging. Possibly electric LI pod auxiliary drive.

New Aquacote paint too, a great help removing the old broken down well known brand from the hull with Bote-Cote's new water based 'Lemon Peel' paint stripper.

(Dreaming of Adelaide to Auckland and return.)

Completely redesigned lighter weight trailer with a new driver initiated braking system, much lowered trailing height, greater ease of access, on the road and at the ramp.

The 'new' boat looks like becoming a worthy successor to *Charlie Fisher*.

The phone amputation still hurts, every Thursday morning.

KEL GOWANS PROTOTYPE NIS 10.5 'BIRDWING' LAUNCHING DECEMBER 12



NIS 10.5 Pix of Kel's *Birdwing* in next issue.

(Somewhere near Lismore...)

Kel and Vicky Gowans have been building their new NIS 10.5 up in Lismore. It's been a big project, and Kel has been imaginative in his own customising ideas and very helpful with our evolution of the new 10.5 kits, which should become available in the Pacific Europe and the North America by late March, joining NIS 5.8. NIS 7, NIS 8 which are already available.

We will be doing a bigger story after the launch, but it's so great to see Kel and Vicky get to this point.

We express our gratitude.

More info?



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Whether learning to sail in an Optimist, perfecting technique in a Laser or taking line honours in the latest sportboat, the ability to visualise apparent wind angle and read the shifts is critical to sailing well. Traditional wind indicators tend to be fragile and easily bent or broken. Davis Instruments introduces Blacksmith Wind Vanes, made from super lightweight 3D carbon fibre and duraluminum to withstand the rigors of hard racing. For Opti sailors, an equally durable, carbon-free class legal version is offered.

Blacksmith Wind Vanes are handcrafted. Calibrated and tested, they're precision-balanced to be incredibly sensitive in breezes from 0.8-40kts. Hi-tech black with yellow highlights, they stand out in both sunny and overcast skies. All models come with an American flag sticker that can be used to identify a US sailor at international competitions.

Two masthead models are offered. The Blacksmith Sport Boat Wind Vane is for boats 4.8m–8.5; it measures 23cm W x 25cm H and weighs a mere 20g. The Blacksmith Olympic is for boats up to 6m, weighs 13.8g and is 23cm W x 23cm H. The vanes and shafts are made from 3D carbon fibre, an incredibly light and strong composite. Duraluminum used on the vanes is a heat-treated alloy renowned for its strength, minimal weight and corrosion resistance. Both include a versatile Davis J-base for mounting on the side, top or front of the mast, the same as comes with a Windex 10, making switching to a Blacksmith easy.

The Blacksmith Horizontal Wind Vane is perfect for dinghies that use gooseneck indicators mounted on the mast at eye level, such as a Laser, Finn or Melges 14. Weighing only 25g, its shaft is 30cm L with a 20cm vane. Made with the same long-lasting materials as the masthead models, it installs with a durable, easily attached adjustable mast clamp.

The Blacksmith Crazy Kids is designed with the Optimist sailor in mind. Class legal, it's totally carbon-free—its vane is made from durable, lightweight fibreglass. It's 18cm W x 28cm H, only 14g and has a fun, kid-friendly pattern imprinted on it.



Contact Davis Instruments
www.davisnet.com

(Clockwise from top) Davis Blacksmith Olympic, Sport Boat, Horizontal and Crazy Kids Wind Vanes



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